

CITY OF ARMADALE

AGENDA

OF DEVELOPMENT SERVICES COMMITTEE TO BE HELD IN THE COMMITTEE ROOM, ADMINISTRATION CENTRE, 7 ORCHARD AVENUE, ARMADALE ON MONDAY, 9 DECEMBER 2024 AT 7.00PM.

A meal will be served at 6:15 p.m.

OFFICIAL OPENING & ACKNOWLEDGEMENT OF COUNTRY

PRESENT:

Dr C M Wielinga (Chair)
Cr M Silver (Deputy Chair)
Mayor R Butterfield
Cr J Keogh
Cr S Peter JP
Cr L Sargeson
Cr G J Smith
Cr S Stoneham

APOLOGIES:

OBSERVERS:

IN ATTENDANCE:

PUBLIC:

DISCLAIMER

The Disclaimer for protecting Councillors and staff from liability of information and advice given at Committee meetings to be read.

DECLARATION OF MEMBERS' INTERESTS

QUESTION TIME

Public Question Time is allocated for the asking of and responding to questions raised by members of the public.

Minimum time to be provided – 15 minutes (unless not required)

Policy and Management Practice EM 6 – Public Question Time has been adopted by Council to ensure the orderly conduct of Public Question time and a copy of this procedure can be found at <http://www.armadale.wa.gov.au/PolicyManual>

It is also available in the public gallery.

The public's cooperation in this regard will be appreciated.

DEPUTATION

CONFIRMATION OF MINUTES

RECOMMEND

Minutes of the Development Services Committee Meeting held on 18 November 2024 be confirmed.

ITEMS REFERRED FROM INFORMATION BULLETIN - ISSUE 20 - DECEMBER 2024

If any of the items listed in the Information Bulletin - Issue 20 – December 2024 require clarification or a report for a decision of Council, this item to be raised for discussion at this juncture.

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DEVELOPMENT SERVICES COMMITTEE

9 DECEMBER 2024

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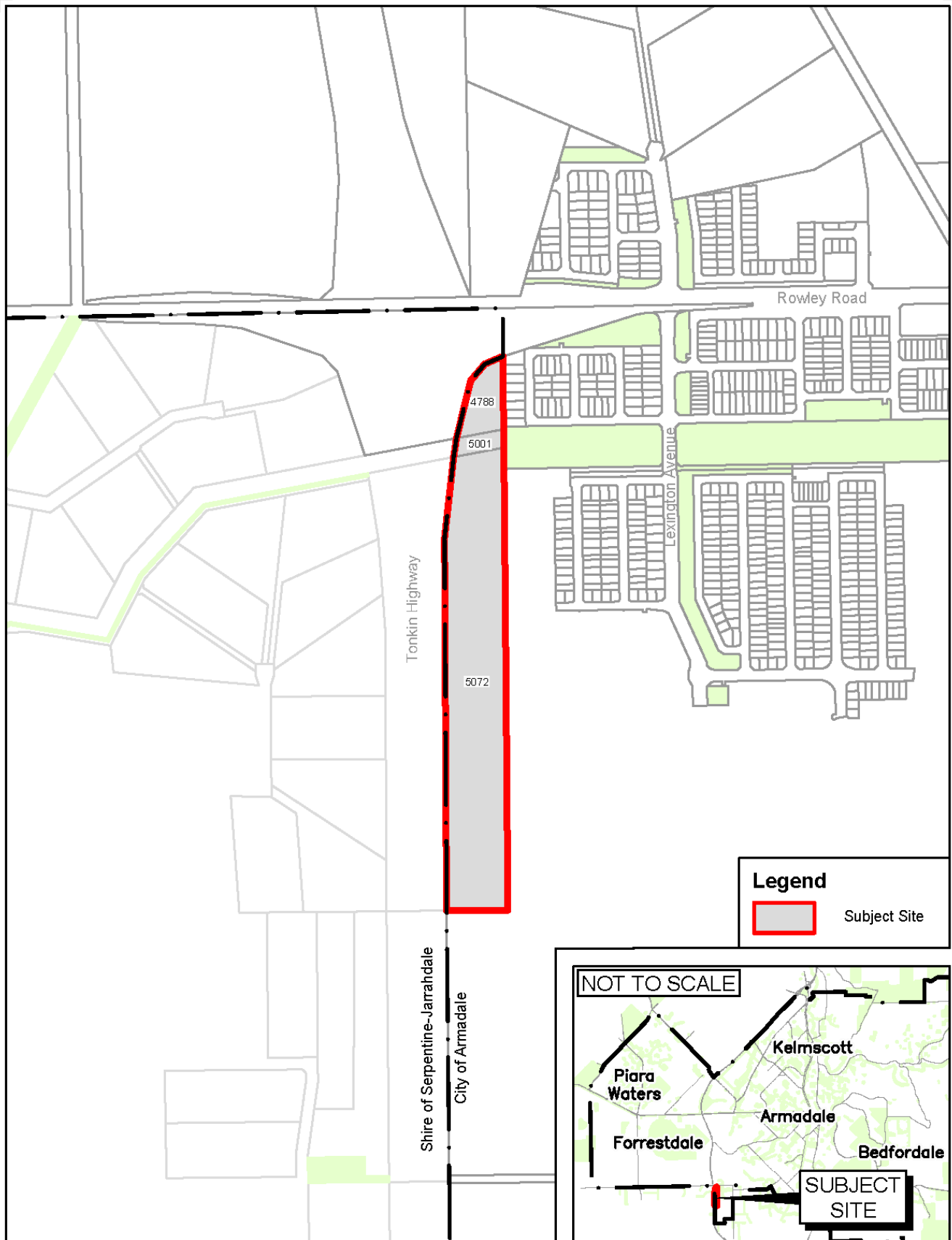
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LOCATION PLAN
Lots 4788 Rowley Road, 5001 and
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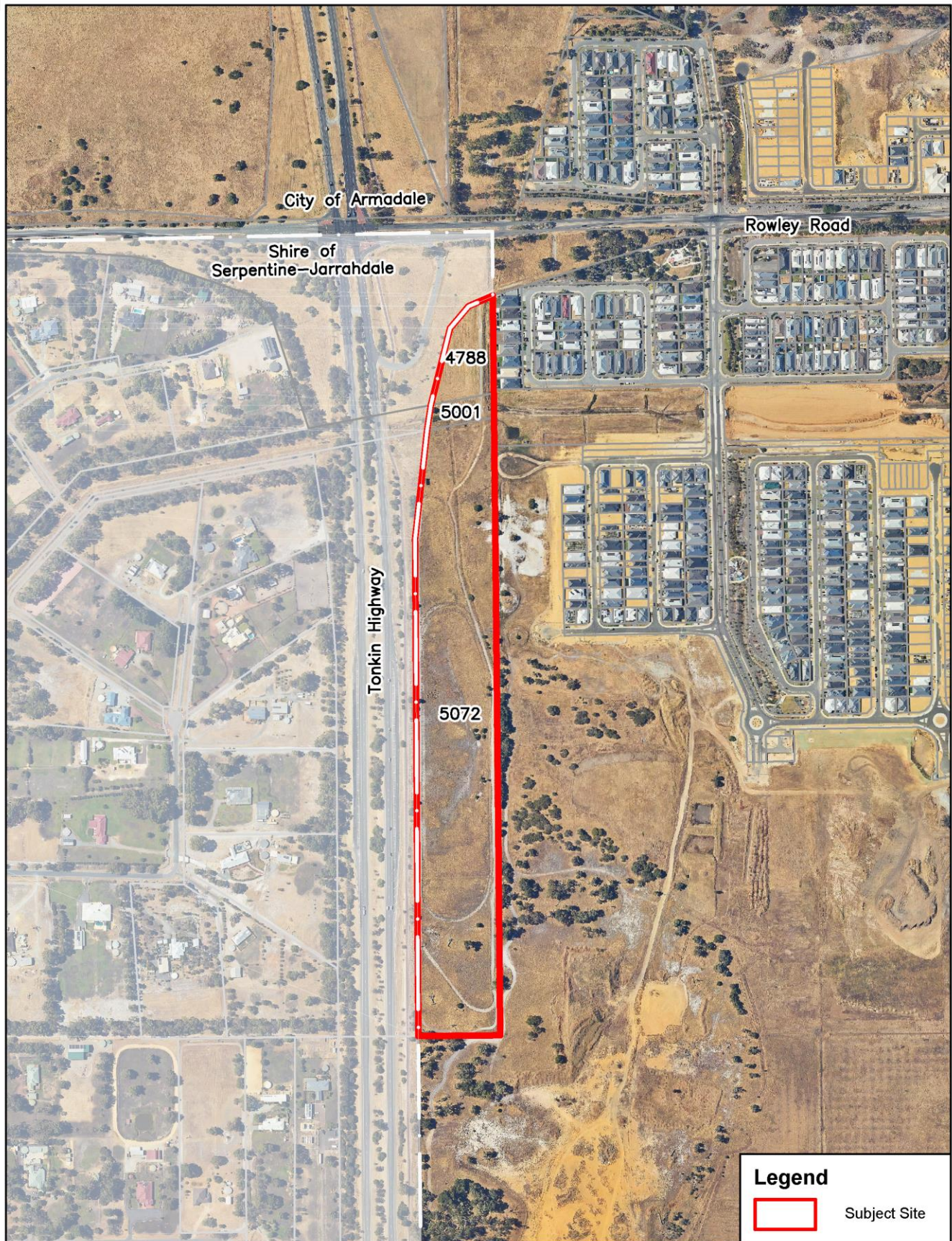


SCALE 1 : 10000

DATE 14 October 2024 - REVISION 2401
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Aerial photograph supplied by LANDSAF, Redmond by ViewMap.





AERIAL PLAN
Lots 4788 Rowley Road, 5001 and
5072 Atlantic Road, Hilbert



SCALE 1 : 7500

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Western Australian Land Information Authority trading as Landgate (2012).
Aerial photograph supplied by Landgate. Photogram by NearMap

***1.1 - CONSIDERATION OF MINISTER'S MODIFICATIONS – TPS NO.4
AMENDMENT 104 – ZONING OF LOT 5072 ATLANTIC ROAD, HILBERT AND LOT
5001 ROWLEY ROAD, HILBERT AND INCLUSION OF LOT 4788 INTO TPS NO.4***

WARD : MINNAWARRA

FILE No. : M/666/24

DATE : 30 November 2024

REF : SN

RESPONSIBLE : Executive Director
MANAGER : Development Services

APPLICANT : City of Armadale

LANDOWNER : Crown Land
John Rossi

SUBJECT LAND : Lot 5072 Atlantic
Road, Hilbert, and
Lots 4788 and 5001
Rowley Road, Hilbert

ZONING
MRS / : Primary Regional Roads
Rural

TPS No.4 : Un-zoned

In Brief:

- At its 22 July 2019 meeting, Council resolved to adopt Amendment 104 to TPS No.4 and to forward the amendment to the Western Australian Planning Commission and the then Hon Minister for Transport and Planning for final approval.
- The Minister for Planning; Lands; Housing; Homelessness has directed the City to modify the Amendment pursuant to Section 87(2) of the *Planning and Development Act 2005*.
- The Minister has required a modification to replace the proposed 'General Rural' zone with the 'Urban Development' zone.
- Recommend that Council adopt the modified Amendment in accordance with the Minister's modifications.

Tabled Items

Nil

Decision Type

- Legislative** The decision relates to general local government legislative functions such as adopting/changing local laws, town planning schemes, rates exemptions, City policies and delegations etc.
- Executive** The decision relates to the direction setting and oversight role of Council.
- Quasi-judicial** The decision directly affects a person's rights or interests and requires Councillors at the time of making the decision to adhere to the principles of natural justice.

Officer Interest Declaration

Nil

Strategic Implications

- 2.5.1 Ensure the City's planning framework is modern, flexible, responsive and aligned to achieving the outcomes of the Strategic Community Plan and Corporate Business Plan.
- 3.3.1 Ensure the City has contemporary strategies and dynamic planning frameworks to be responsive to economic development opportunities and trends.

Legal Implications

Planning and Development Act 2005

Planning and Development (Local Planning Schemes) Regulations 2015

Metropolitan Region Scheme (MRS)

Town Planning Scheme No.4

Council Policy/Local Law Implications

Local Planning Strategy 2016

Budget/Financial Implications

Nil

Consultation

The July 2019 report to Council includes the previous consultation for Amendment 104.

BACKGROUND

Initiation and Adoption

Town Planning Scheme No.4 (TPS No.4) Amendment No.104 was initiated by Council at its 25 February 2019 meeting and adopted at its 22 July 2019 meeting. The Amendment was then forwarded to the Western Australian Planning Commission (WAPC). The background and planning considerations applicable to Lot 5072 Atlantic Road, Hilbert and Lot 5001 Rowley Road, Hilbert are included in those reports.

The proposed Amendment No.104 at adoption was to:

1. modify the City's TPS No.4 Scheme boundary to transfer all of Lot 5072 Atlantic Road and Lots 4788 and 5001 Rowley Road, Hilbert within the scheme boundary; and
2. zone Lot 5072 and 5001 as "General Rural".

Metropolitan Region Scheme Amendment 1380/57

The landowner requested that this site be rezoned to the Urban zone to facilitate the structure planning and development of this site for residential purposes.

In July 2021, the Western Australian Planning Commission (WAPC) sought public comment on the above-mentioned amendment to the Metropolitan Region Scheme (MRS). The Minister approved the amendment as modified by the WAPC. The above-mentioned amendment to the MRS Scheme took effect on and from Friday, 23 December 2022, being the date of publication in the Government Gazette to zone Lot 5072 Atlantic Road, Hilbert and Lot 5001 Rowley Road, Hilbert to Urban under MRS.

DETAILS OF PROPOSAL

This report outlines the City’s assessment of the extent of each modification (e.g. minor or substantial) and a recommendation to Council on subsequent progression of the Amendment. A copy of the modified Amendment text and the Amendment Plan that the Minister requires Council to resubmit to the Minister for approval is attached.

Minister’s Decision

The WAPC have now written to the City advising that the Hon Minister for Planning; Lands; Housing; Homelessness requires modifications to the Amendment in accordance with Clause 87(2)(b) of the *Planning and Development Act 2005* as follows:

1. Modify the Amendment Resolution Text Item 1b) by replacing “General Rural” with “Urban Development”.
2. Modify the Proposed Zoning Map to replace the “General Rural” over Lots 5001 and 5072 with “Urban Development”.

Due to the MRS Amendment No. 1380/57 and the direction of the Minister’s modifications relating to Lot 5072 Atlantic Road, Hilbert and Lot 5001 Rowley Road, Hilbert, it is appropriate for Council to consider the modified Amendment text and Map changes, as required by the Minister.

The below table has been prepared to summarise the Council’s adopted Amendment proposal, describe the Minister’s required modifications and provide recommendations of the City’s Planning Services Department on the substantiality of the modification.

TPS No.4 A 104 Adoption July 2019 meeting Council Resolution	Minister’s Modification	Minister’s Justification
1.b) Modify the Scheme Map to zone Lot 5072 Atlantic Road, Hilbert and Lot 5001 Rowley Road, Hilbert to ‘General Rural’.	<ol style="list-style-type: none"> 1. Modify the Amendment Resolution Text Item 1b) by replacing “General Rural” with “Urban Development”. 2. Modify the Proposed Zoning Map to replace the “General Rural” over Lots 5001 and 5072 with “Urban Development”. 	<p><i>“Lots 5001 and 5072 were recently rezoned from Rural to Urban under the Metropolitan Region Scheme. Accordingly, the local planning scheme needs to be in conformity with that zone and the Urban Development zone is the most appropriate.”</i></p>

The Minister’s Schedule of Modifications (including Plan) as presented in the Attachments to this report.

COMMENT

The City of Armadale Amendment No. 104 proposes to include Lot 5072 Atlantic Road, Hilbert and Lot 4788 and Lot 5001 Rowley Road, Hilbert within TPS No.4 and zone the Lots 5001 and 5072 as General Rural. However, Lots 5001 and 5072 have now been zoned Urban under the Metropolitan Region Scheme (MRS) and therefore the proposed General Rural zone is no longer consistent with the MRS zoning which is a requirement of the Planning and Development Act 2005.

The City raised concerns with the area changed to Urban without Schedule 8 provisions forming part of the amendment and the area not being shown in the Special Control Area (SCA) Map 3 with a development area number. Schedule 8 provisions are there to help guide structure plans and subdivision development proposals for an area. The change to Urban would allow the lot to be developed for residential, which would involve the structure plan process. This would be consistent with how the area has been developing through the Wongong Urban Water Masterplan, specifically Precinct J Structure Plan located to the east which is currently being developed for residential purposes.

The Department of Planning Lands and Heritage (DPLH) Officers, on behalf of the Minister and the WAPC, have provided explanatory justification for the required modifications in response to the City's comments, which is summarised below:

Schedule 8 – Structure Plan requirements

City Comment

Further to the Ministers request for modifications, the City advises that a structure plan will be required to be prepared by the landowner for the area. As such, in order to conform with the City's TPS No.4, the area also needs to be identified as a Development Area on Special Control Area Map 3 and have provisions relating to the requirement to prepare a structure plan (including relevant matters to be dealt with in the structure plan) being included in Schedule 8 of TPS No. 4.

DPLH Response

The Urban Development zone requires a structure plan by virtue of Clause 3.2.10 of the City of Armadale Town Planning Scheme No. 4 (TPS4). The Zoning Table ties land use permissibility in the Urban Development zone to the specific zones identified on the Structure Plan in accordance with Part 4 of the Deemed Provisions.

In addition, Clause 4E.1.1 requires subdivision and development to be in accordance with an approved Structure Plan prepared in accordance with Part 4 of the Deemed Provisions. In this regard it is not considered necessary that the land also be designated as a Development Area on Special Control Area Map 3 in order for a structure plan to be required.

Clause 4.12.2 of TPS4 supports this as it states, *“In an area where comprehensive planning is required, a Structure Plan may be required prior to the local government's support for subdivision, notwithstanding the area has not specifically been defined as a Development Area.”*

Development Contribution Plan

City Comment

The adjoining estate (which is under the control of DWA) has a Developer Contribution Plan (DCP) and the City will have to consider whether a DCP would be required for Amendment 104.

DPLH Response

The subject land does not need to be included in TPS4 as the City has this ability to consider whether a DCP would be required regardless. Should a DCP be contemplated for the land it will need to be subject to an amendment to TPS4.

ANALYSIS

Council has been directed by the Minister under Section 87(2)(b) of the *Act* to make modifications to Amendment No.104.

(2) *The Minister may, in relation to a local planning scheme or amendment submitted to the Minister under subsection (1) —*

(b) require the local government concerned to modify that local planning scheme or amendment in such manner as the Minister specifies before the local planning scheme or amendment is resubmitted for the Minister's approval under this subsection;

There are no formal appeal rights if Council is aggrieved by the changes required. Notwithstanding the lack of appeal rights, it is open for Council to consider political advocacy if Council so determines. Should Council wish to make a submission to the Minister with respect to some or all of the modifications required, then that option is available.

The modification to the MRS means that Amendment 104 has now transitioned straight to Urban Development and has forgone the rezoning process and relevant studies aligned with that process. Therefore, these issues will now need to be addressed by the applicant at the Structure Plan and subdivision stages. This is not a sound approach to progressing land development and usually results in longer more complex processes at these stages. It is noted that the applicant has lodged a Structure Plan and the City's officers have identified some of these matters and decided to not accept lodgement of the Structure Plan pending resolution of outstanding matters.

The developer will be required to landscape the public open space areas as part of the land subdivision process.

Council now needs to consider the requested modifications and resolve to adopt the modified text and zoning map in full or in part and send the Amendment back to the WAPC and the Hon Minister for final approval.

OPTIONS

1. Council may acknowledge and endorse the modifications requested by the Minister and authorise the Mayor and CEO to execute the modified amendment documentation prior to returning the documentation for final approval and gazettal without making any further submissions to the Minister.
2. Council may lodge a submission seeking the Minister's reconsideration and explaining why Council is aggrieved by the changes required by the Minister and await the Minister's response prior to implementing modifications to Amendment No.104 and authorising the Mayor and CEO to execute the modified amendment documentation.

CONCLUSION

As a consequence of the area being rezoned under the MRS from 'Rural' to 'Urban', the Urban Development zoning under the TPS4 is necessary to be consistent with the MRS. Accordingly, any future development of the site will require the approval of a Structure Plan to further guide any development and subdivision of the land.

In accordance with Option 1 above, it is recommended that Council resolve to endorse the modifications requested by the Minister, authorise the Mayor and CEO to execute the modified Amendment documentation and return the documentation for final approval and gazettal, without making any further submissions to the Minister.

RECOMMEND

That Council:

1. **Notes the Minister for Transport, Planning and Ports' direction pursuant to Section 87(2) of the *Planning and Development Act 2005* to require the implementation of modifications to Amendment No.104 prior to the Amendment receiving final approval; and**
2. **Authorise the Mayor and CEO to execute the modified Scheme Amendment documentation and return the documentation for final approval and gazettal, without making any further submissions to the Minister.**

ATTACHMENTS

1. [↓](#) TPS4 Zoning Map
2. [↓](#) Ministers Schedule of Modifications



AERIAL PLAN

Stationmaster's House
Lot 1091 (No. 2793) Albany Highway, Kelmescott



SCALE 1 : 1000

DATE 22 November 2024 - REVISION 2401
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Western Australian Land Information Authority trading as Landgate (2012).
Aerial photograph inspired by Landgate. Photography by HeatMap



1.2 - PROPOSED ADDITIONS AND ALTERATIONS TO STATION MASTER'S HOUSE- LOT 1091 - 2793 ALBANY HIGHWAY KELMSCOTT

WARD : RIVER
FILE No. : M/772/24
DATE : 29 November 2024
REF : DT
RESPONSIBLE MANAGER : Executive Director
Development Services

In Brief:

- DevelopmentWA (DWA) has referred a Development Application for the Kelmscott Station Master's House to the City for comment.
- The proposal involves works to instal toilet facilities, additional security measures, replacement of some internal materiality and repair works.
- It is recommended that Council supports the application subject to conditions related to amenity and heritage outcomes.

Tabled Items

Nil

Decision Type

- Legislative** The decision relates to general local government legislative functions such as adopting/changing local laws, town planning schemes, rates exemptions, City policies and delegations etc.
- Executive** The decision relates to the direction setting and oversight role of Council.
- Quasi-judicial** The decision directly affects a person's rights or interests and requires Councillors at the time of making the decision to adhere to the principles of natural justice.

Officer Interest Declaration

Nil

Strategic Implications

- 2.3 Diverse and attractive development that is integrated with the distinctive character of the City.
2.3.1 Provide supportive planning and development guidance and liaison on major land developments
- 2.4 Attractive and user-friendly streetscapes and open spaces
2.4.1 Implement townscape, streetscape and parkland improvements to enhance the distinctive character of the City.

Legal Implications

Planning and Development Act 2005

Planning and Development (Local Planning Schemes) Regulations 2015

Metropolitan Region Scheme (MRS)

Armadale Redevelopment Scheme 2

Town Planning Scheme No.4

Council Policy/Local Law Implications

Local Planning Strategy 2016

Local Heritage Survey (LHS)

Budget/Financial Implications

Nil

Consultation

The City has advocated to the State Government for the Station Master's House to be used. Although the City has been invited to offer comment on the proposal now, it is noted that there was no preapplication engagement with the City's officers on the proposed building works, prior to lodgement of the Development Application by the Public Transport Authority (PTA).

The City's officers have referred the development application to the Community Heritage Advisory Group (CHAG). The CHAG's comments will be provided to Councillors following receipt of their advice.

BACKGROUND

The Kelmscott Station Master's House, built in 1905 is a rare example of early 20th-century railway architecture in Western Australia, representing the development of the railway system that was crucial for the region's growth. Its design reflects the typical residential style for railway officials of that era and is part of the broader development of the Kelmscott area.

The building is included/listed in the Metropolitan Redevelopment Authority (now DWA) and City's Heritage Inventories/Local Heritage Survey (see Attachments). The City's Local Heritage Survey (LHS) includes the Old Station Master's House in Appendix 1 with 11 other sites within the DWA's Redevelopment Area for possible future inclusion in the LHS once the DWA's jurisdiction has been normalised.

Despite periods of vacancy since 2014, ongoing discussions about restoring and repurposing of the building have occurred. Security of the building has been an ongoing issue.

The building was partly refurbished as part of the Metronet's Denny Avenue Level Crossing Removal Project to largely stabilise the heritage values. This work included replacement of the roof, windows and ceilings; reconfiguration of verandahs; removal of out-of-character cabinetry, air conditioning and electrical systems; and groundworks to remove soil buildup away from the buildings edge.

Following the works to improve the site, an expressions-of-interest by the PTA in February 2022 to lease the building for offices, shop and retail purposes received no successful responses.

Council has resolved to enter into an Memorandum of Agreement (MOA) with PTA for the management of the Plaza landscaping. The City has requested the PTA to submit a suitable MOA for the City's consideration and make the payment of the agreed landscaping maintenance and landscaping costs, prior to commencing maintenance of the Plaza.

DETAILS OF PROPOSAL

The PTA has lodged a Development Application to DWA for alterations to the building to prepare the building for a commercial use. The application has subsequently been referred to the City for comment by DWA.

The proposal includes the following:

- Placement of a demountable universal access toilet attached to the northern side of the building with services and bin storage being located behind a wooden batten structure screening from public view.
- A *Disability Discrimination Act* compliant access ramp with edge barriers, handrails and tactiles to the western verandah and stairs and handrails added to the eastern verandah.
- Internal office fit out of the building, such as the replacement of internal doors into offices, and installation of carpet in office and meeting rooms.
- Sealing and insulation of internal fireplaces. It is noted that there is no proposal to alter the material structure of the chimney stacks.
- Wire mesh security screening added to each north/south window.
- Continuous security screens to securely enclose the eastern and western verandahs and decorative screening of the verandahs to height of 900mm.
- Closing of the window to the former wood store and replacement of door hardware.

The application also includes details of restoration works to damaged downpipes, metal works and possible repair to existing west facing verandah, however these are exempt from the requirement for development approval in the DWA's Redevelopment Area.

COMMENT

The DWA did not advertise the development application for public comment. The City requested that the DWA advertise the development application for comment and also to extend the City's comment period to enable consideration by Council. Although DWA have extended the City's referral period beyond the statutory 42 day period to allow Council consideration of the proposal and comment is due on 18 December 2024, the proposal will not be advertised for public comment.

DWA has advised that the application has not been advertised as it was deemed not to meet the criteria for advertising under Section 5.13 (2) of the *Armada Redevelopment Scheme 2*. In particular, the works were considered to be minor in nature and not have "negative impact on amenity due to the bulk and scale or design of the development".

DevelopmentWA Armadale Heritage Inventory

DWA maintains its own Heritage Inventory for areas currently under planning control of the Authority. The Station Master's House is located within Precinct 9 – Kelmscott of the Armadale Redevelopment Scheme area and is the sole heritage entry for this area.

The Inventory lists the site as having a Management Level of Level 2 – Place of Local Significance, and notes that the site's historic value is linked to its construction by "the Public Works Department under the State's then Chief Architect, George Temple Poole".

The City's Local Heritage Survey (LHS)

The City's LHS identifies the Old Station Master's House ('Residence') in Appendix 1 which are intended to be included in the LHS when the area is normalised back to the City by DWA. Council's decision in July 2019 (D44/7/19) includes a clause noting that "a further report will be made seeking the inclusion of places in Appendix No 1" in the LHS at the time of normalisation of the area into the City's Local Planning Scheme.

The City's LHS identifies additional historical significance above and beyond that of the DevelopmentWA. The listing notes the symmetrical front verandah to the front elevation, and notes that the site is said to have "aesthetic value as the last remaining dwelling in Western Australian that was built to house a station master." The LHS identifies Management Category 1 and Level of Significance "Exceptional".

ANALYSIS

- *Proposed toilet and storage area:*
 - The installation of a demountable toilet to the northern side of the main structure is proposed to provide amenities to the occupants of the building. The toilet block will not provide any public access.
 - The proposed toilet consists of a single prefabricated universally accessible toilet cubicle directly abutting the northern end of the western verandah. The unit will measure approximately 2.5m by 2.5m by 2.4m tall, be connected to the reticulated sewerage system and be fully screened by a wooden look batten fence. The fence will be offset from the demountable unit to create a screened storage area for bins and other services.
 - The application states that the proposed demountable unit allows for a heritage sensitive response to the internal structure of the building, whilst still providing fully accessible toilet facilities.

It is considered that, although the structure is stylistically and structurally separate for the main heritage elements, the lack of information of the materials to be used makes it difficult to determine whether the screening will be in character with the building. This can be mitigated by specifying a condition for using locally appropriate hardwood.

- *Ramps and access:*

- The proposal includes the installation of ramp and stair access to the western verandah. The height of the brick walls supporting the ramp will not be higher than the finished floor level of the verandah deck, ensuring no visual obstruction to the building from the station.
- The proposed screening to both verandahs include gated access. On the western side this would remain open during business hours, however the eastern gate serves as an emergency exit only and it is proposed that it could not be opened from the outside.

The ramps and stairs to the west present a historically sensitive solution to ensuring compliance with the *Building Code of Australia* and *Disability Discrimination Act* access requirements. However, the gate on the northern side of the development would remain open during trading hours. This could present a security issue by allowing open access to the toilet facilities (and subsequent public use) and by creating areas within the ends of the verandah where a person could be entrapped without alternative means of escape, contrary to Crime Prevention Through Environmental Design principles. The open nature of a mesh screen would help ameliorate this risk by creating open site lines.

- *Security Measures:*

- The proponent states that the installation of security screening to the verandahs and the use of stainless-steel mesh screens to sash windows are required due to ongoing vandalism of windows and external walls. The proponent advises that all proposed new structures are self-supporting and can be readily removed with no adverse impact to heritage fabric.

It is considered that the enclosure of the verandahs contradicts prior work to re-open the former lean to additions at the rear of the property. The verandah at the rear is proposed as bicycle and/or general storage and represents a possible back of house space that would be visually open to the southern side of the station forecourt. Due to landscaping works associated with the Denny Ave Level Crossing Removal, this is one of the most prominent views of the structure. If used for storage, this area could become unsightly and if not used for this purpose, the space could become an unactivated void. With addition of the one way gate, this side of the property could not be used for ingress or display of goods if a retail use was entertained.

The addition of half-height balustrading to the verandahs is not aligned to the architectural style of the construction period. The built form of Federation housing was marked by iconic basic verandah with minimal decoration. The ballustrading appears to be added to minimise the prominence of the steel mesh barriers and prevent users from walking into them. It is noted that ballustrading is not required by the *Building Code of Australia* for decked structures with a height of less than 1m.

Given this, it is recommended that the security elements be reconsidered to better align them with period architecture.

- *Fireplaces:*

- The proposal indicates that the four fireplaces remaining in the building would be sealed and insulated.

In relation to the fireplaces, it is recommended that the mantels and an open firebox structure be retained by sealing using a throat damper and sympathetically capping the chimney.

- *Outbuilding (Old Wood Store) modifications:*

- The proposal indicates that the windows to the old wood store would be closed over with a solid panel and a new door with a viewing panel installed in place of the former hardware.

This portion of the proposal relates to creating secure storage and is a response to broken windows in the outbuilding. The closure of the window represents a significant diversion from the original format of the building, though this matter is not included in the Heritage Impact Statement.

It is recommended that the window be protected with mesh screening similar to that on the sash windows and verandah.

OPTIONS

The options include:

1. Council could resolve to advise DWA of its support the Development Applications with no conditions.
2. Council could resolve to advise DWA of its support the Development Application subject to conditions.
3. Council could resolve to advise DWA that it does not support the Development Application.

CONCLUSION

It is recommended that the proposal be supported subject to conditions that relate to the reworking of security measures to minimise architectural and visual impact; the preservation of the integral visual elements of the fireplaces; and specification of a material palette for the battens surrounding the toilet structure to minimise its visual impact.

The proposed internal works to the building are generally in line with conservation principles. However, it would be preferable to ensure that the material elements of the fireplaces are conserved and a condition to facilitate this is recommended.

The proposed screened temporary toilet structure provides a suitable solution that ensures toilet facilities can be provided without impacting the internal heritage structure. In as far as the materials of the battens are appropriate, a clear distinction between old and new should be made.

The proposed security measures on the verandahs and the outbuilding provide an unsatisfactory aesthetic outcome that is not consistent with the architectural style of the original structure. Conditions to require removal of the balustrading and to not cover windows in the outbuilding are recommended.

RECOMMEND

That Council:

- 1. Advise DevelopmentWA that the City supports the application, subject to the following conditions:**
 - (i) The balustrading to the front and rear verandahs to be removed from the plans, with mesh security screen to extend the full height of the verandahs.**
 - (ii) The fireplaces are sealed using a method that preserves the firebox structure and any mantelpieces to the satisfaction of DevelopmentWA, on the advice of the City of Armadale.**
 - (iii) The slat material surrounding the toilet be constructed of treated native hardwood timber, or a similar material.**
 - (iv) That windows to the outbuilding shall not be covered, but instead be treated with the same security mesh as the sash windows of the main building.**

ATTACHMENTS

1. [↓](#) Referral Letter
2. [↓](#) Application Cover Letter
3. [↓](#) Proposed Plans
4. [↓](#) MRA Armadale Heritage Inventory - Place Record Form
5. [↓](#) City of Armadale Local Heritage Survey Entry
6. [↓](#) Heritage Impact Statement

2.1 - PIA PLANNING CONGRESS DARWIN 28-30 MAY 2025

WARD : ALL
FILE No. : M/609/24
DATE : 2 December 2024
REF : RB
RESPONSIBLE MANAGER : Executive Director
Development Services

In Brief:

- The Planning Institute of Australia (PIA) Planning Congress 2025 will be held at the Darwin Convention Centre in Darwin NT, on the 28 – 30 May 2025.
- Matters to be covered should be of relevance to Armadale.
- Recommend that consideration be given to nominating a Councillor to attend.

Tabled Items

Nil.

Decision Type

- Legislative** The decision relates to general local government legislative functions such as adopting/changing local laws, town planning schemes, rates exemptions, City policies and delegations etc.
- Executive** The decision relates to the direction setting and oversight role of Council.
- Quasi-judicial** The decision directly affects a person's rights or interests and requires Councillors at the time of making the decision to adhere to the principles of natural justice.

Officer Interest Declaration

Nil.

Strategic Implications

Relates to the aim for good governance and leadership to:

- 4.1 Visionary Civic leadership and sound governance
4.1.3 Support the role of the elected body
4.1.3.3 Advocate and support Councillor engagement in outside bodies and events to maintain awareness and enhance the City's knowledge base.

Legal Implications

Nil.

Council Policy/Local Law Implications

Council Policy ADM3 – Conferences and Training
Council Policy EM1 – Reimbursement of Councillor's Expenses

Budget/Financial Implications

The PIA Planning Congress is on the City's approved Conference List for both Officers and Elected Members. Officer attendance will be funded from the Development Service's Conference and Meeting budget. Sufficient funds are available in the Councillor/ Member Development Budget should a Councillor be nominated to attend. The cost of the conference is estimated at \$5,000 per delegate.

BACKGROUND

The PIA is holding its 2025 Planning Congress at The Darwin Convention Centre in Darwin NT, from the 28 – 30 May 2025. Study Tours will be held on Wednesday 28 May 2025. However, as the program is still being developed, this could be subject to change.

The PIA Planning Congress typically hosts a number of topics which are potentially applicable to the City and may provide valuable insight and information regarding a number of areas that impact the City.

Nominations for Councillor attendance are requested at this Development Services Committee Meeting, to be presented to the December 16th Ordinary Council Meeting, in order to facilitate timely registrations.

Committee is reminded of Elected Member and Chief Executive Officer Professional Development Policy clause (c) states that: -

“(c) Where a member of Council at the date of the conference, seminar or training course has an electoral term of less than 6 months to complete, such member shall be ineligible to attend unless such is specifically authorised by Council”.

DETAILS OF CONFERENCE

The 2025 PIA Congress will feature leading national and international experts and practitioners discussing how we, as a nation, respond to the challenges for the planning profession and the broader community in our major cities through to our regional centres. It will discuss technical development, policy development and personal development. For people and companies working in the built-environment sector – be it urban and regional planning, land development, urban design, transport planning, economic development, environmental sustainability and social planning.

Keynote speakers and the full program are yet to be announced. [Planning Congress website is available.](#)

CONCLUSION

An Officer may be attending the 2025 PIA Planning Congress. The matter is brought to the attention of Council to determine if it wishes to nominate a Councillor to attend.

RECOMMEND

That Council:

- 1. Nominates Cr _____ to attend the PIA Planning Congress will be held at the Darwin Convention Centre, on the 28 – 30 May 2025;**
- 2. Approves leave of absence for Cr _____ for attendance at the PIA Planning Congress from 28-30 May 2025.**

or

- 3. If there is no nomination at the Development Services Committee or the Ordinary Meeting of Council on 25 November 2024, then the recommendation be as follows:**

That no nomination be made for an Elected Member to attend the PIA Planning Congress will be held at the Darwin Convention Centre, on the 28 – 30 May 2025.

ATTACHMENTS

There are no attachments for this report.

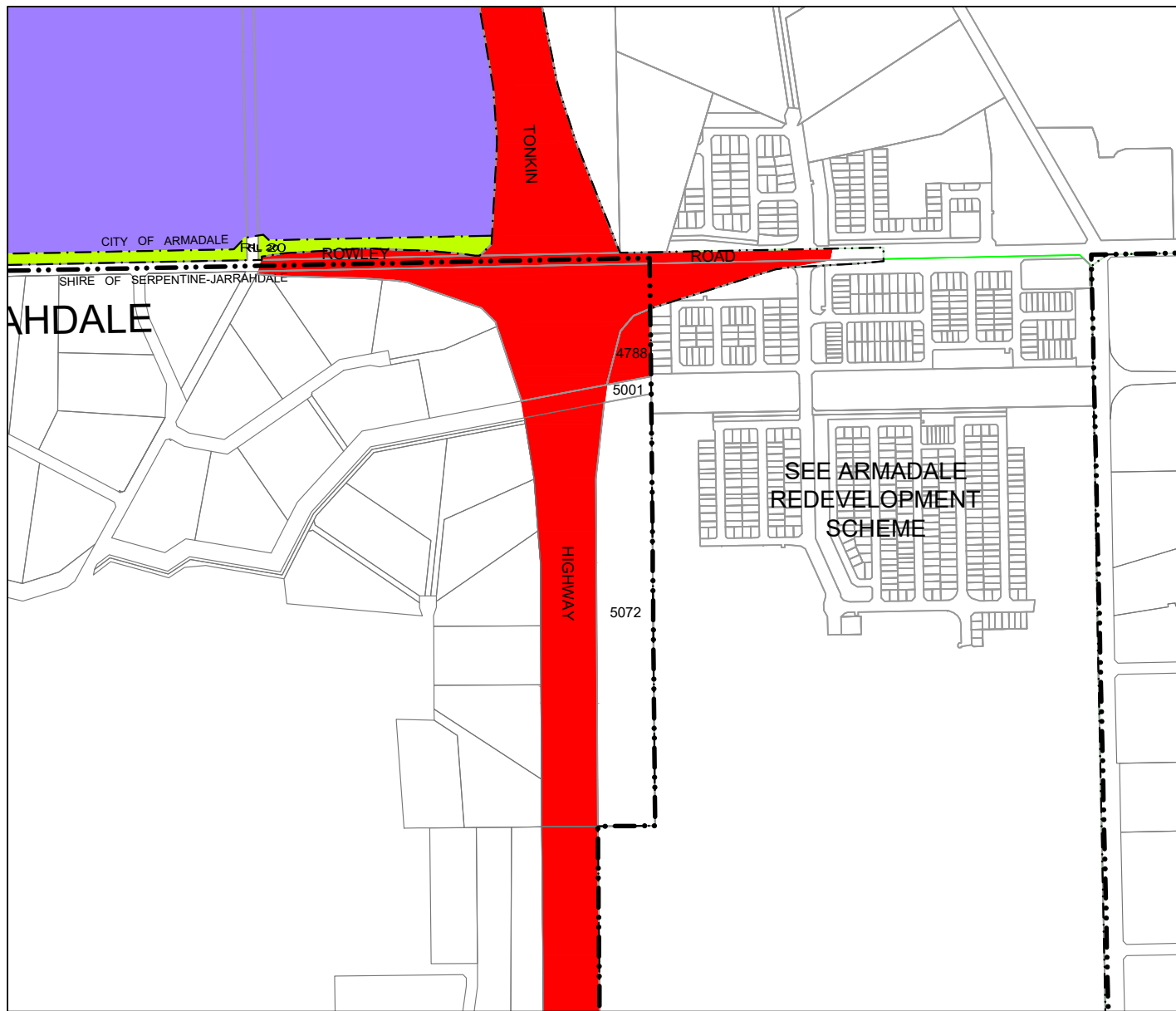
COUNCILLORS' ITEMS

EXECUTIVE DIRECTOR DEVELOPMENT SERVICES REPORT

MEETING DECLARED CLOSED AT _____

DEVELOPMENT SERVICES COMMITTEE		
SUMMARY OF ATTACHMENTS		
9 DECEMBER 2024		
ATT NO.	SUBJECT	PAGE
1.1 CONSIDERATION OF MINISTER’S MODIFICATIONS – TPS NO.4 AMENDMENT 104 – ZONING OF LOT 5072 ATLANTIC ROAD, HILBERT AND LOT 5001 ROWLEY ROAD, HILBERT AND INCLUSION OF LOT 4788 INTO TPS NO.4		
1.1.1	TPS4 Zoning Map	25
1.1.2	Ministers Schedule of Modifications	26
1.2 PROPOSED ADDITIONS AND ALTERATIONS TO STATION MASTER'S HOUSE- LOT 1091 - 2793 ALBANY HIGHWAY KELMSCOTT		
1.2.1	Referral Letter	28
1.2.2	Application Cover Letter	29
1.2.3	Proposed Plans	40
1.2.4	MRA Armadale Heritage Inventory - Place Record Form	47
1.2.5	City of Armadale Local Heritage Survey Entry	51
1.2.6	Heritage Impact Statement	57

EXISTING ZONING



ZONES

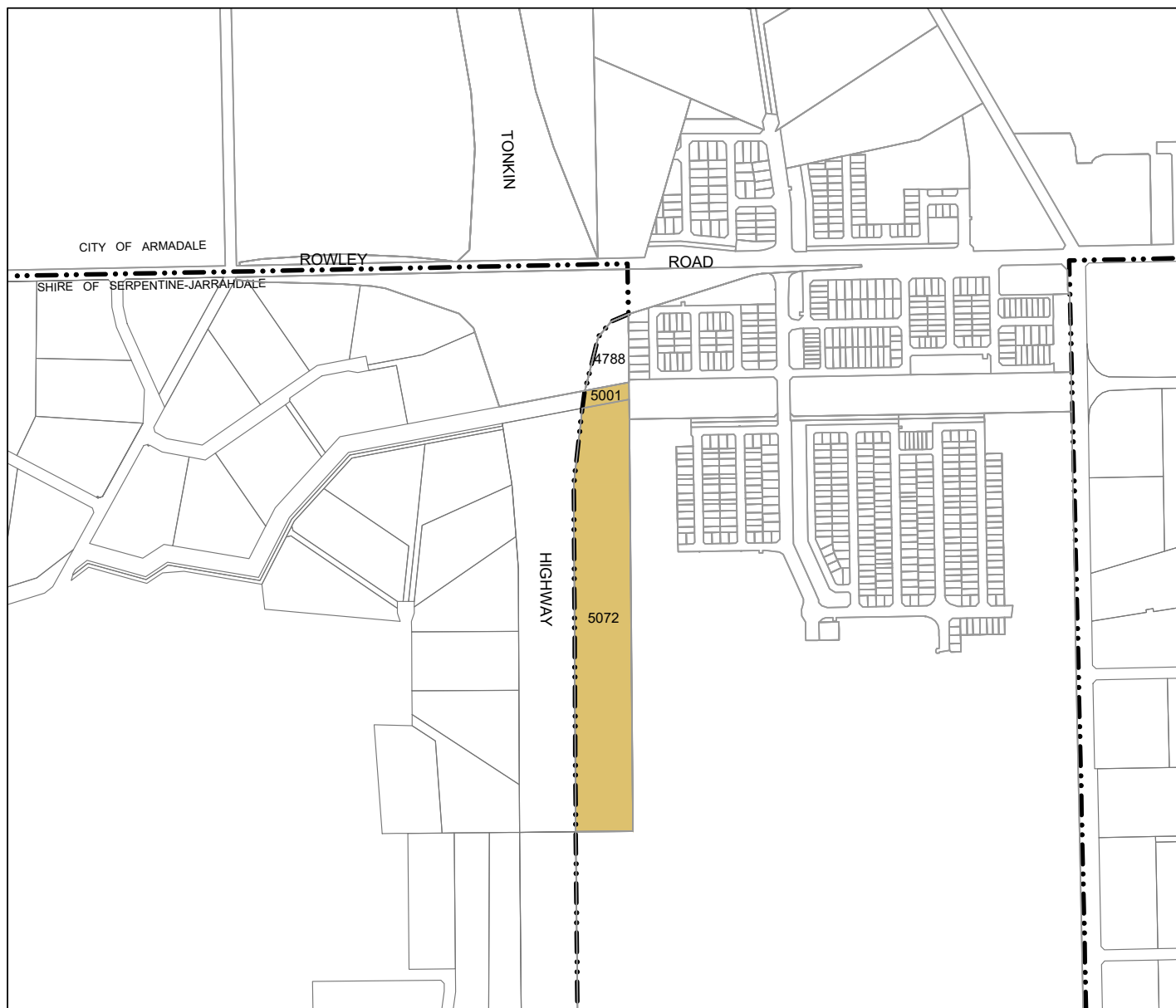
- RESIDENTIAL Including Residential Planning Density Codes
- URBAN DEVELOPMENT ZONE
- RURAL LIVING - 1,2,4,10,20,X
- GENERAL RURAL
- SPECIAL RURAL
- SPECIAL RESIDENTIAL
- MIXED BUSINESS / RESIDENTIAL
- LOCAL CENTRE
- DISTRICT CENTRE
- STRATEGIC REGIONAL CENTRE
- GENERAL INDUSTRY
- INDUSTRIAL BUSINESS
- INDUSTRIAL DEVELOPMENT
- SPECIAL USE
- ADDITIONAL USE
- RESTRICTED USE
- RESTRICTED USE
- DENSITY CODE BOUNDARY
- SCHEME BOUNDARY

RESERVATIONS

- PARKS & RECREATION (Region)
- PARKS & RECREATION (Local)
- RAILWAYS
- STATE FOREST
- WATERWAYS
- WATER CATCHMENTS
- PRIMARY REGIONAL ROADS
- OTHER REGIONAL ROADS
- PUBLIC PURPOSE (Local)
- PUBLIC PURPOSE (Region) Denoted as Follows
 - Commonwealth Government
 - Technical School
 - High School
 - Primary School
 - Hospital
 - Water Authority of WA
 - Special Use
 - State Energy Commission
 - Telstra

NOTE: The reservations are shown diagrammatically and do not purport to represent the road reservations shown in the Metropolitan Region Scheme and information in respect to road widening requirements for these roads should be obtained from the Department of Planning

PROPOSED ZONING



AMENDMENT No. 104

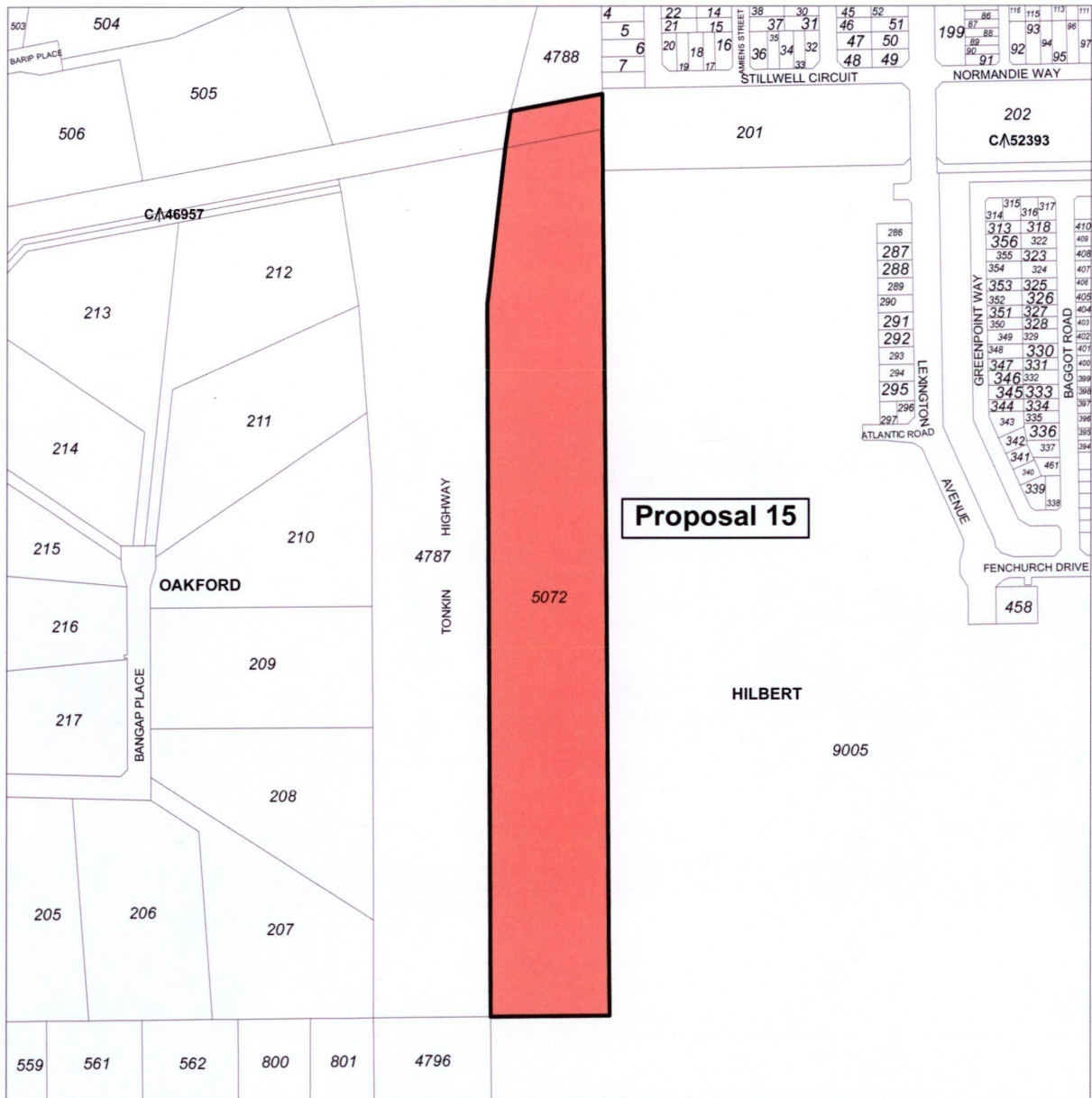
City of Armadale Town Planning Scheme No. 4



ATTACHMENT 2

**SCHEDULE OF MODIFICATIONS
CITY OF ARMADALE
TOWN PLANNING SCHEME NO. 2 AMENDMENT NO. 104**


NUMBER	PROPOSED MODIFICATION	JUSTIFICATION
1	Modify the Amendment Resolution Text Item 1b) by replacing 'General Rural' with 'Urban Development'.	Lots 5001 and 5072 were recently rezoned from Rural to Urban under the Metropolitan Region Scheme.
2	Modify the Proposed Zoning Map to replace the 'General Rural' over Lots 5001 and 5072 with 'Urban Development'.	Accordingly, the local planning scheme needs to be in conformity with that zone and the Urban Development zone is the most appropriate.



South East and South West Districts Omnibus Amendment No. 4
Proposed minor amendment
as advertised

9 December 2020

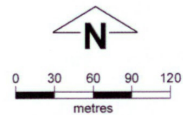
Proposal 15

Proposed Amendment:
 Urban zone

Oracle reference no: 3010
 File number: 833/02/01/0074
 Version number: 4



Date: 25/11/2020
 Produced by Data Analytics, Department of Planning, Lands and Heritage, Perth WA
 Base information supplied by Western Australian Land Information Authority SLIP 1096-2018-1





Our Ref: MRA-14537 (A2939289)
Enquiries: Daniel Iversen (6200 4312)
Date: 30 October 2024

Mr Glen Windass
Manager Statutory Planning
City of Armadale
VIA EMAIL: info@armadale.wa.gov.au

Dear Glen

**APPLICATION FOR APPROVAL TO UNDERTAKE DEVELOPMENT
LOT 1091 (NO. 2793) ALBANY HIGHWAY, KELMSCOTT
PROPOSED ADDITIONS AND ALTERATIONS TO COMMERCIAL BUILDING**

Please find attached plans of the above-mentioned development application for your consideration and comment.

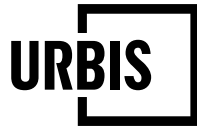
In order to facilitate the timely assessment of the application, it is requested that any comments on the application be provided at your earliest convenience, by no later than 20 November 2024, being 21 days from the date of this letter.

Should you have any queries or concerns in relation to the application or referral process please contact to Daniel Iversen, Planning Officer on 6200 4312 or via email daniel.iversen@developmentwa.com.au at the earliest opportunity.

Yours sincerely

Cheyenne Ellis
A/Manager Statutory Planning

Att: Development Plans and Form 1 Development Application



LEVEL 8
1 WILLIAM STREET
PERTH WA 6000

URBIS.COM.AU
Urbis Ltd
ABN 50 105 256 228

15 October 2024

Planning Team
DevelopmentWA
Level 7, 5 Spring Street
Perth WA 6000

To whom it may concern,

DEVELOPMENT APPLICATION - KELMSCOTT OLD MASTER STATION HOUSE (HERITAGE BUILDING) – LOT 1091 ON PLAN 796

We write to request approval of a development application that falls within the Armadale Redevelopment Area, and approval is to be sought from DevelopmentWA. Urbis on behalf of the Public Transport Authority is excited to submit the following development application regarding the revitalisation of the Existing Old Station Masters Residence, located at Lot 1091 on Plan 000796.

Please find an assessment of the relevant framework below.

To lodge this Development Application please find the following documents:

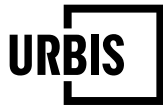
- MRA Form 1, signed by the landowner and applicant.
- Planning Cover Letter and Assessment.
- Certificate of Title and Management Order. **(Appendix A)**
- A copy of the proposed development plans. **(Appendix B)**
- Heritage Impact Statement. **(Appendix C)**

SITE DETAILS

Table 1 below outlines the details of the subject site. This demonstrates the Public Transport Authority has management over the subject site. Refer to Appendix A for a copy of the Title and Management Order.

Table 1 Site Details

Lot on Plan	Reserve No.	CT	Ownership	Management Order
Lot 1091 on P000796	22555	LP3136/672	State of WA	Public Transport Authority



SCOPE OF WORKS

The proposed application includes the following works to a heritage building, however, proposes minor modifications to the external of the building, promoting the heritage fabric of such building, consistent with Development Policy 2 – Heritage:

- Minor internal office fit out of the Station Master Building, such as the replacement of internal doors into offices.
- Wire mesh security screening added to each north/south window, which is minimal on visual impact.
- Continuous security screens to both verandas (East and West) to stop vandalism of windows and the external of the heritage building.
- Decorative screening of the verandas (Battens to 900H, topped with a sill metal mesh panel between 900H and the underside of the roof).
- Inclusion of a demountable universal access toilet attached to the North of the building with services and bin storage being located behind an additional structure screening from public view.
- Restoration of damages downpipes, metal works and possible repair to existing west facing veranda. (Exempt from development approval)
- DDA access ramp with edge barriers, handrails and tactiles to west veranda and stairs added to east veranda, including handrails.

The overall intent of the works is to provide security upgrades and compliance works, to ensure the building meets universal access requirements. This in turn will facilitate the occupation of the building by a future tenant for long term use.

ARMADALE REDEVELOPMENT SCHEME 2

The Armadale Redevelopment Scheme 2 (the Scheme) provides the overarching legislation for the Armadale Redevelopment Area, and divides the scheme area into 4 project areas, of which the subject site is located within the Kelmscott Project Area. The defined vision for the area is primary focused on delivering a vibrant town centre with a high level of active connectivity through the town centre promoting a transit-oriented development area for workers, residents and visitors.

The proposed development immediately responds to the vision by increasing workers within proximity to the new train station, as well as providing high quality and attractive design ultimately achieving the main goal of redeveloping the project area. The internal works to the Station Masters House will also activate this heritage building, which has been vacant for a number of years.

Considering the above, it is considered that the proposed development is consistent with the vision and objectives of the Scheme.

CHAPTER 6 – LAND USE

The proposed use remains unchanged with that is currently approved on the site. The Armadale Heritage Inventory states that the site is “*currently used as a commercial office*” however currently vacant, and no change of use is required. The ‘office’ land use is considered a contemplated use within the Kelmscott Town Centre.



KELMSCOTT TOWN CENTRE PRECINCT DESIGN GUIDELINES

The subject site is located within the Station/Civic Sub-Precinct which aims to provide an attractive and active context for the station upgrade and a civic focus for Kelmscott. The Guidelines identify focus areas with the site located in Focus Area A and meets the proposed objectives to provide surveillance and encompasses the Plaza of Kelmscott and its connection to the train station.

As the building is existing, majority of the guidelines don't apply to this application, however the proposal enhances surveillance, minimises visual effects of services equipment and revitalises a heritage building, which is considered compliant with the Design Guidelines.

DEVELOPMENT POLICY 2 – HERITAGE PLACES

Development Applications within the redevelopment area are required to be consistent with the principles and provisions of the Scheme as well as the performance standards of the Development Policy 2.

A requirement of the Policy includes the submission of a Heritage Impact Statement (HIS) which has been provided at **Appendix C**. This HIS concludes that the works generally comply with Development Policy 2 and can be readily removed in the future with no adverse physical impact to the places noted heritage fabric or values.

A summary assessment of the performance standards as below, explaining compliance with the development policy.

Table 2 – Performance Standards

Performance Standard	Provided
<p>P1. Heritage significance</p> <ul style="list-style-type: none"> Development should respect the recognised cultural heritage significance, statement of significance and level of management recorded for the heritage place or heritage precinct in the Heritage Inventory and/or the State Register of Heritage Places.; and Development does not adversely affect, damage or destroy heritage significance 	<p>The works to the Heritage Building recognises the Level 2 – Place of Local Significance which are afforded to the highest level of protection. This proposal acknowledges that and respects the heritage while not affecting or damaging the significance.</p>
<p>P2. Quality new design</p> <ul style="list-style-type: none"> New work should respect the context, scale, setback, materiality and character of the original. The key to success is carefully considered design of a high quality that respects and supports the significance of the place. It should be readily identifiable as new development, should not imitate or replicate the heritage architecture 	<p>The proposal is mostly considered to apply to an existing building envelope and any proposed structures are consistent with the significance of the site.</p>



Performance Standard	Provided
<p>nor should it detract from the cultural heritage significance of a heritage place or heritage precinct.</p>	
<p>P3. Adaptive re-use</p> <p>The heritage place continues to be used for the purpose for which it was built, or for a use with which it has a long association. Where this is not considered possible, adaptive re-use is to be undertaken.</p>	<p>N/A. The application proposes the same use/purpose as it was built for/is under the heritage Inventory.</p>
<p>P4. Extensions and additions (including additional structures)</p> <p>Extensions and additions to heritage places are compatible and complementary to the place and are designed to achieve:</p> <ul style="list-style-type: none"> • The continued retention and protection of the heritage significance of the heritage place. • Compatibility in terms of proportions, height, setbacks, materials, colours and other details of the heritage place. The visibility of additions, especially where these are of a height greater than that of the original building, will be sensitively located to reduce visual impact. • The continued preservation of existing important views, sightlines and setting. • Where possible, works that are reversible / removable to avoid permanent damage to the significant fabric of the original building; and • Minimal impact on any significant trees, landscape elements or site features. 	<p>The one extension is consistent with the performance standard, and effectively screens service equipment from public view. The addition effectively fits into the built form consistent with the verandas.</p>
<p>P5. Demolition</p> <p>Heritage places are conserved and maintained to a high standard wherever possible, and partial or complete demolition of heritage places is only approved in exceptional circumstances</p>	<p>N/A – No demolition is proposed.</p>



Performance Standard	Provided
<p>P6. Development adjacent to heritage places and precincts</p> <p>New buildings should not undermine the significance or detract from the prominence or character of adjoining Heritage Places.</p>	<p>N/A – the proposal is not a new building.</p>
<p>P7. Environmental sustainability devices</p> <p>Alterations to heritage places to achieve energy efficiency or other sustainability devices are compatible and complementary to the place and have been designed to:</p> <ul style="list-style-type: none"> • minimise the impact of renewable energy systems on the cultural significance of heritage buildings. • minimise impact on the visual setting of the heritage place, particularly of its main frontage and the dominant roof. • not detract from the visual presentation of the place to its primary street or significant vista. • all necessary electrical and plumbing conduits for the system are located so as to prevent visual clutter; and • the system must be able to be removed. 	<p>As mentioned above, services are effectively screened from view minimising the visual impact and the screening devise is consistent with the development. Additionally, they can be removed if needed.</p>

The policy outlines criteria that can be exempt from requiring development approval including internal works (fit out) and maintenance work or like for like replacements of materials of a heritage building. We respectfully request that the fit out and repair/replacement works of this application be exempt from planning approval, as stated in the Scope of Works.

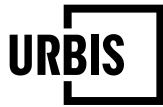
DESIGN REQUIREMENTS AND CONSERVATION

This section of the Heritage Policy details the conservation and design for internal and external alterations and additions to heritage places. The table explores the compliance with the relevant Design Conservation Requirements were appropriate.



Table 3 – Heritage Conservation Design

Conservation Requirements	Proposed
<p>Verandas and Shopfronts</p> <ul style="list-style-type: none"> • All original or historic verandas, awnings and shopfronts on heritage places are to be retained and conserved. • For both repairs and accurate reconstruction (based on detailed evidence only), any replacement material should generally match the original/existing; and • Where replacement of a veranda is required and evidence does not exist of the original, a simple veranda/shopfront of the general form, dimensions and materials of the period should be constructed. Historic detailing should not be replicated 	<p>Verandas and awnings have been retained and conserved.</p>
<p>Roofs</p> <ul style="list-style-type: none"> • The original form of the roof should be maintained. Any repair work is to be consistent with both the existing material and construction method. • Gutters, fascias, soffits, barges, finials, original rainwater heads and downpipes, are to be retained and conserved where practical to do so. Any replacement should be undertaken in a like-for-like manner; and • Placement, design and size of new elements are to be carefully considered, especially when these elements affect heritage views. 	<p>The roof of the building will be retained.</p>
<p>Chimneys</p> <ul style="list-style-type: none"> • Original chimneys are to be retained and conserved. Materials used in reparation are to be consistent with existing materials. • Where chimney pots are missing or badly damaged, they are to be replaced to match those remaining. • Where a chimney is missing it should be reconstructed if detailed evidence exists of its material, design, etc. Where little or no evidence exists, a simply designed chimney that takes its material, design and scale from that of the building should be constructed. 	<p>The chimneys are to be retained and conserved.</p>
<p>Walls</p>	<p>Walls are proposed to be repaired and maintain in a similar material to match the</p>



Conservation Requirements	Proposed
<ul style="list-style-type: none"> • Additions or repairs to walls are to be undertaken in a material to match that of the original, with consideration given to colour, texture, composition, dimensions and detailing. • Replacement of materials should be carefully matched to the original. For bricks special consideration should be given to matching colour, texture, dimensions, bonding pattern, mortar colour and content. For stone, consideration should be given to matching durability, composition, colour, texture, size, and the original source. Replacement timber should match the species, water content, dimensions and profiles of the original and be of a high quality to ensure longevity. If replacing painted woodwork (to be finished in paint), it may be acceptable for the wood to be of a different species, however it must be of a high-quality wood.; and • Cleaning and restoring of external walls is to follow professional advice for the conservation or restoration to original condition. 	<p>original heritage value, as required for building compliance.</p>
<p>Security Screens</p> <ul style="list-style-type: none"> • Installation of aluminium security screens or other heavy style screens is generally unacceptable, however treatments such as transparent films and some stainless-steel mesh screens may be acceptable if they are applied with restraint, have a minimal visual impact and are installed internally. 	<p>The security screens proposed on the veranda and windows are required as windows and external walls are being vandalised and this option is the only way to prevent this.</p>
<p>Windows</p> <ul style="list-style-type: none"> • Timber-framed windows are to be repaired rather than replaced if they are largely sound and intact. • Original or early windows and window details including frame, architrave, sash, glass, glazing bars, hardware, and shutters should be retained and repaired. Where replacement is essential, new work should match the original in terms of style, materials, detailing and dimensions. • The creation of new window openings to principal façades and elevations to be avoided. New openings, where permitted, should be a sympathetic design but clearly distinguishable as a contemporary alteration; and 	<p>Windows are to stay in current condition.</p>



Conservation Requirements	Proposed
<ul style="list-style-type: none">• Filled in or previously damaged window openings may be reinstated if these windows are made to match historic profiles, shapes, dimensions, details and materials.	
<p>Doors</p> <ul style="list-style-type: none">• Original or early doors should be retained and repaired. Where replacement is essential and no evidence exists of the original, a modern door of the general form, dimensions and materials of the period, but without historic detailing, should be installed.• Original or early hardware and door surrounds should be retained and repaired as necessary. Historic reproduction detailing or hardware should not be added to doors or architraves where none existed.; and• The creation of new door openings to principal façades and elevations should be avoided. Any new door, where permitted, should be sympathetically designed, should not detract from the features of the Heritage Place, and be clearly distinguishable as a contemporary alteration.	Existing front and rear doors are to be kept as part of this application

CONCLUSION

In conclusion, the proposed additions and heritage alterations to the existing Old Station Master House will provide significant uplift within the Kelmscott Project Area providing meaningful revitalisation to Kelmscott.

Overall, the development is largely compliant and consistent with the relevant planning framework above. Therefore, the proposal is worthy of planning approval, subject to fair and reasonable conditions.

If you have any questions, please don't hesitate to contact the undersigned.

Kind regards,

A handwritten signature in black ink, appearing to read "Rebecca Travaglione".

Rebecca Travaglione
Associate Director
+61 8 9346 0524

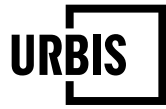
rtravaglione@urbis.com.au



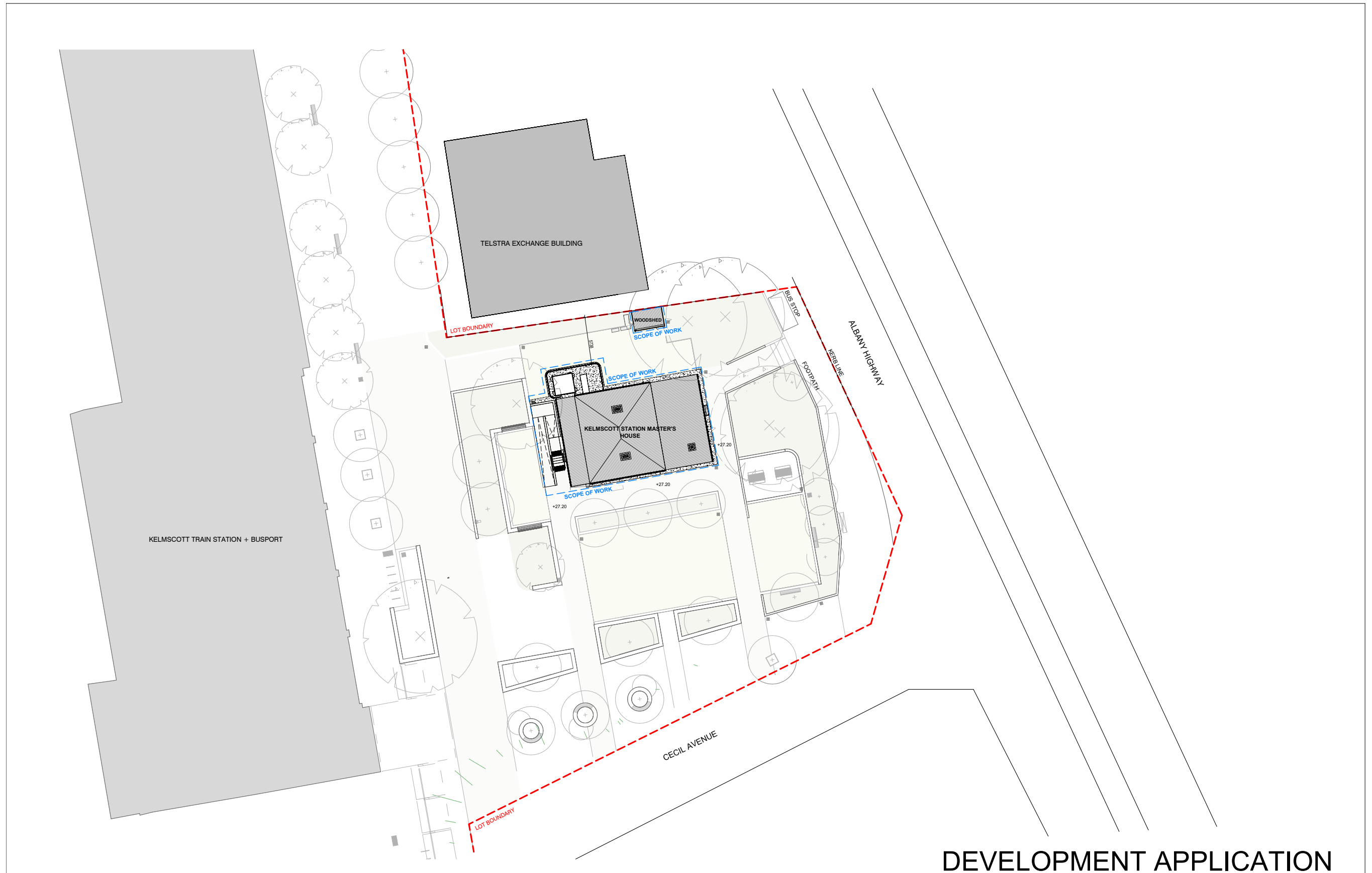
APPENDIX A - CERTIFICATES OF TITLE AND MANAGEMENT ORDER



APPENDIX B – DEVELOPMENT PLANS

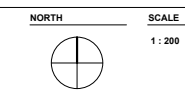


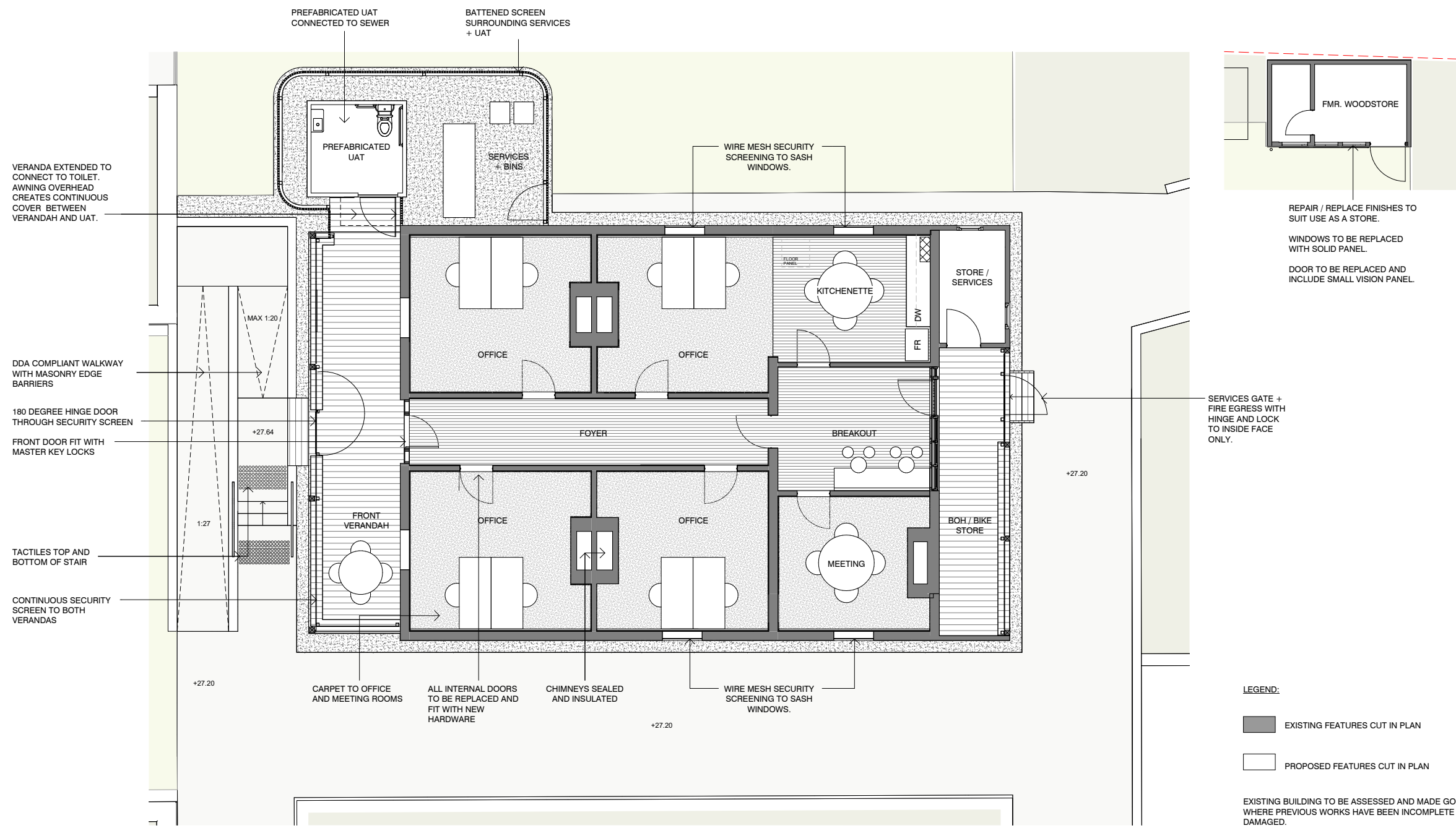
APPENDIX C – HERITAGE IMPACT STATEMENT



DEVELOPMENT APPLICATION

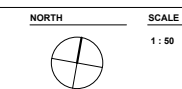
PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	SITE PLAN

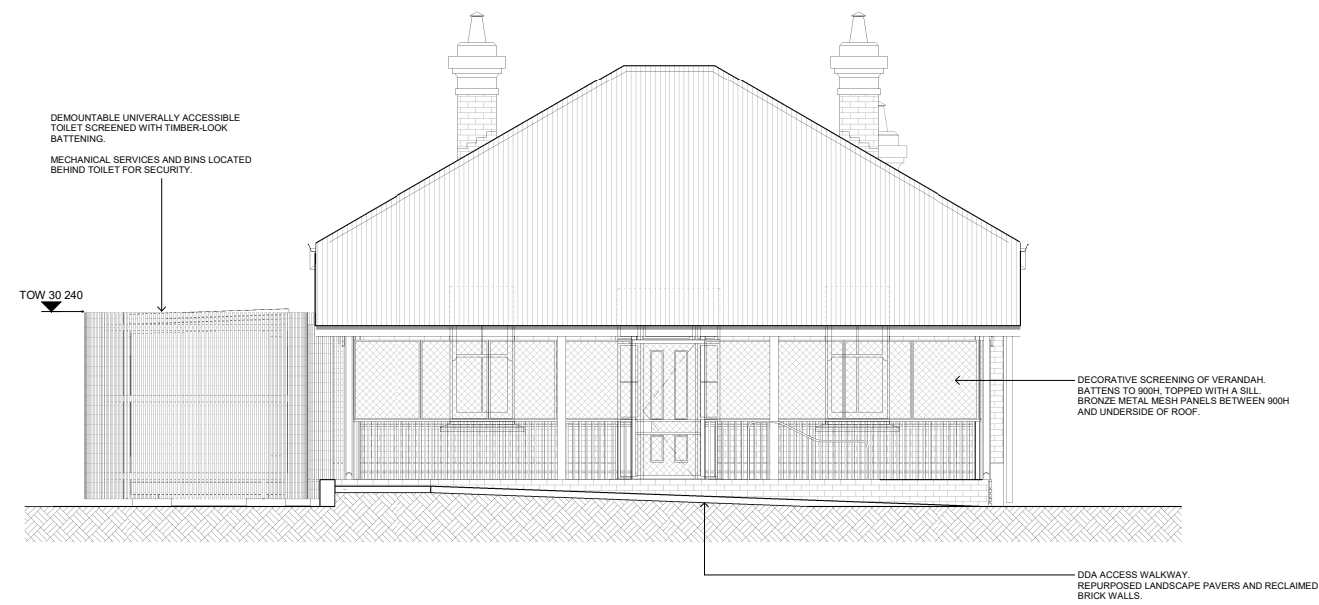




DEVELOPMENT APPLICATION

PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	FLOOR PLAN





WEST ELEVATION



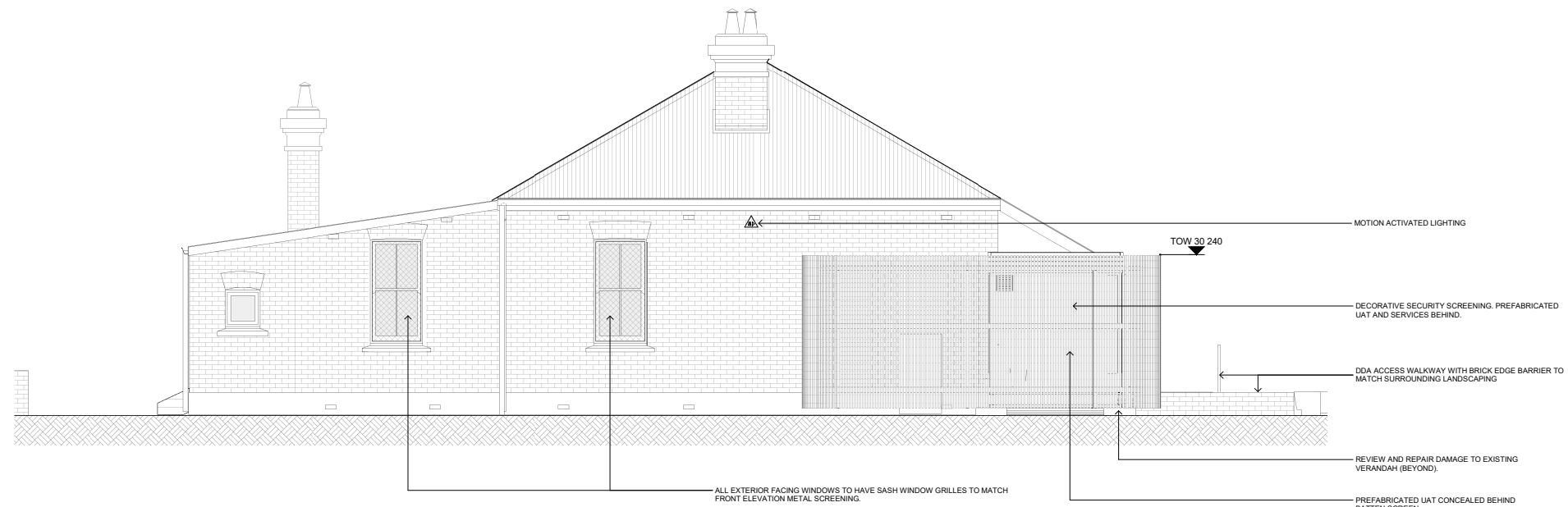
EAST ELEVATION

DEVELOPMENT APPLICATION

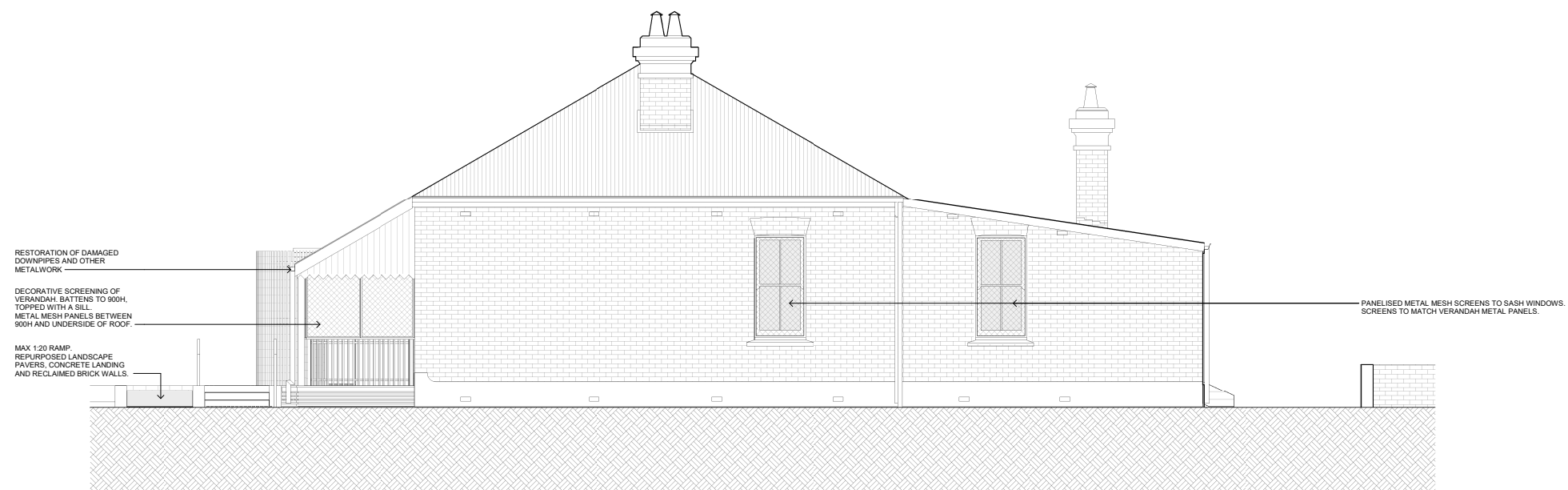
PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	ELEVATIONS

NORTH
SCALE
1 : 50





NORTH ELEVATION



SOUTH ELEVATION

DEVELOPMENT APPLICATION

PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	ELEVATIONS

NORTH SCALE
1:50





DEVELOPMENT APPLICATION

PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	3D VIEW

NORTH SCALE





DEVELOPMENT APPLICATION

PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	3D VIEW

NORTH SCALE





DEVELOPMENT APPLICATION

PROJECT	JOB NUMBER	DATE	DRAWING
KELMSCOTT TRAIN STATION MASTER'S HOUSE RENOVATION	80640	15/10/2024	3D VIEW

NORTH SCALE



MRA Armadale Heritage Inventory - Place Record Form

Old Station Masters Residence

MRA Ref No. K1 / HCWA Place No. 03479

Other Names: N/A



FIGURE 15: THE STATION MASTERS RESIDENCE, AS SEEN FROM KELMSCOTT RAILWAY STATION ENTRY ROAD (2016)

MANAGEMENT LEVEL

Level 2 – Place of Local Significance

LOCATION

Address: No. 2793 Albany Highway, Kelmscott
Land title description: Lots 1091, on Plan 796, bounded by Albany Highway (to the east) and Streich Avenue (to the south)
Scheme area: Armadale Redevelopment Scheme
Precinct: Precinct 9 - Kelmscott

STATEMENT OF SIGNIFICANCE

The Station Masters Residence is the last remaining dwelling in Western Australia that was built to house a station master and is one of the few remaining structures outside of Perth related to the Perth to Armadale railway line opened in 1893. It has historic and social significance, built by the Public Works Department under the State's then Chief Architect, George Temple Poole.

OTHER HERITAGE LISTINGS

City of Armadale Municipal Heritage Inventory 1995

DESIRABLE OUTCOME

These places are included in the State Register of Heritage Places and will be afforded the highest level of protection through the provisions of the Heritage Act, the Redevelopment Scheme and Heritage Development Policy.

HISTORICAL NOTES

Date of Construction: c.1905
Original Use: Dwelling - Kelmscott Station Masters residence
Current Use: Vacant

HISTORICAL NOTES

Brief History: Following the granting of Responsible Government to Western Australia in 1890, greater levels of government borrowing were available to fund public works. The South Western Railway, which included a line from Perth to Armadale, was part of the funded works, with the line opening in 1893. The Kelmscott station was opened in 1893, originally consisting of timber station and a goods yard to the rear.

The need for a residence to house the station master was only determined in 1898, with a tender for the Kelmscott and Armadale (S.W.R) Station Master's Houses Contract advertised by the State Railway Department on 24 February 1898, which included the Kelmscott Station.

The Station Masters Residence was completed in 1905, with Mr T L Warner becoming the first Station Master to occupy the dwelling. The resident Kelmscott Station Master and Officer in Charge occupied the dwelling until 1970.

Since the site ceased being a residence, the site has been used for a variety of the commercial purposes, with former tenants including a real estate agency and an op-shop.

PHYSICAL DESCRIPTION

The place comprises a single-storey brick and iron residence, currently used as a commercial office. Features include a corrugated iron roof incorporating a verandah across the front elevation, masonry chimneys and timber frame windows. The walls of the building are constructed in red face brick, with contrast render to the base of the walls and window sills.

Additional structures have been installed to the original structure, including aluminium security screens. A small brick and iron shed is located in the rear yard of the property, originally used as the external toilet. Two original brick chimneys are located towards the rear of the building.

The dwelling originally consisted of the three bedrooms, dining room, lounge, kitchen and bathroom, opening up onto a rear verandah. Two of the bedrooms had brick fire places, with additional fire places on the other side of the internal wall to the lounge and dining room. Since the site's discontinuance as a residence, modifications of the internal layout have been made, with several internal walls removed.

Several established gum trees are located on the site, to the rear of dwelling, adjacent to the Albany Highway road reserve.

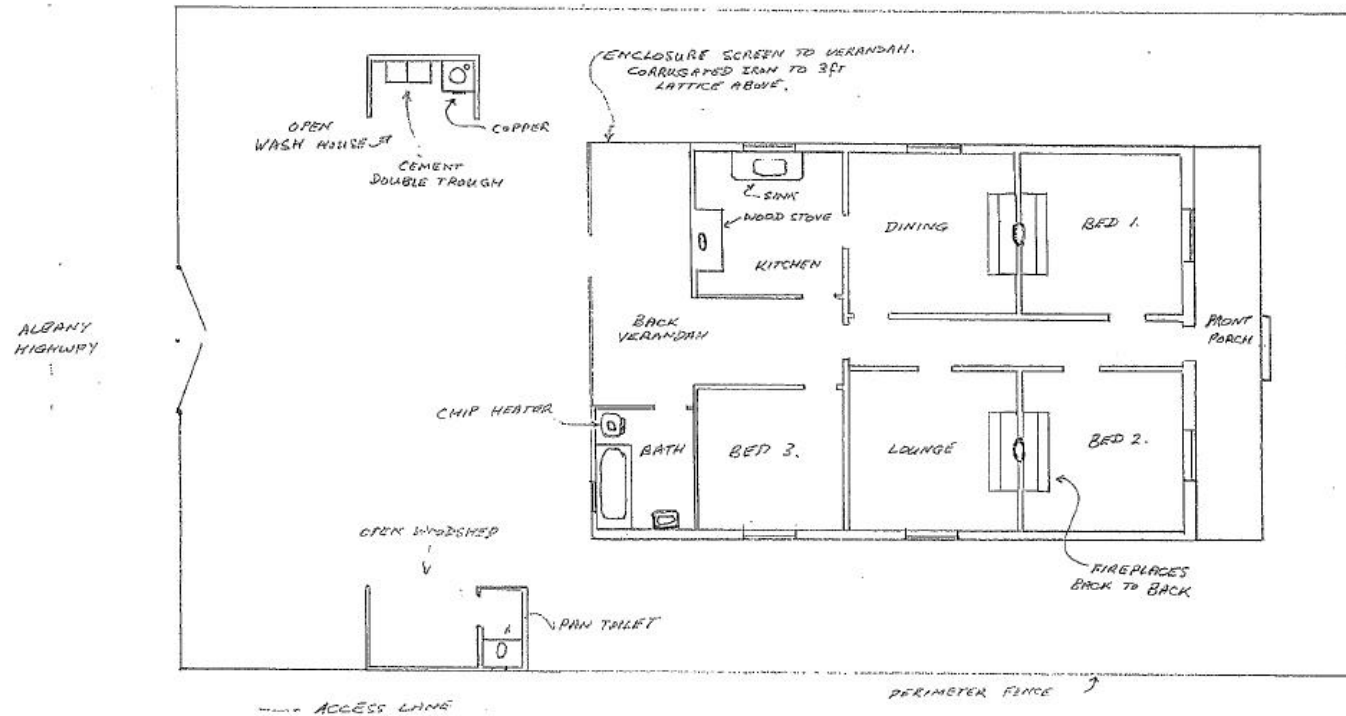
Additional fixtures to the building include two external air-conditioning condenser outlets (visible from the western elevation on the roof and wall of the northern elevation) and gas hot water system (visible from southern elevation). Openings on the southern elevation of the building (fronting Albany Highway) have been boarded up to prevent vandalism and illegal occupation of the site.

REFERENCES AND/OR OTHER SOURCES OF INFORMATION

City of Armadale (1995). Municipal Heritage Inventory 1995

Heritage and Conservation Professionals (2006) Kelmscott Town Centre – Cultural Heritage Explanatory Report

"South Western Railway - Kelmscott and Armidale (sic) Station Masters' Houses Contract", The West Australian (Perth, WA -1879-1954), p.2, available from <http://trove.nla.gov.au/newspaper/article/3196591> (accessed 25 February 2016)



THIS STATION HOUSE: KELMECOTT 1950.

FIGURE 16: INTERNAL PLAN OF THE KELMECOTT STATION MASTERS RESIDENCE, 1950 (SOURCE: ARMADALE DISTRICT HERITAGE GROUP)



FIGURE 17: OLD STATION MASTERS RESIDENCE, AS SEEN FROM THE REAR ELEVATION, 2005 (SOURCE: HERITAGE AND CONSERVATION PROFESSIONALS)



Old Station Master's Residence

MRA
 Appendix 1



SITE INFORMATION	
Other Names	Kelmscott Station Masters House
Street Address	2793 Albany Hwy
Locality	Kelmscott
Landgate PIN	344883
Land Information	Lot 1091 Vol/Folio
	Plan/Diagram P000796
GPS	-32.113492, 116.013870

HERITAGE LISTING	
DPLH inHerit Database	03479
Previous AMHI	-
Other Listings	MRA Armadale Heritage Inventory 2017

PLACE TYPE	
Original Use	Individual Building or Group
Current Use	Transport/Communications - Rail: Housing or Quarters
Other Use	Vacant/Unused
	-



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Appendix 1

CONSTRUCTION DETAILS	
Construction Date	c1905
Walls	Face brick
Roof	Corrugated steel
Architectural Style	Federation Bungalow

PHYSICAL DESCRIPTION

The site is bounded by Albany Highway to the east, Streich Avenue to the south, a Western Power property to the north and an access road for Kelmscott Train Station parking to the west. The building's front elevation faces west.

The place comprises a single-storey brick residence that is currently unoccupied. Features include a corrugated steel roof incorporating a verandah across the front elevation, masonry chimneys and timber framed windows. The walls of the building are constructed in red face brick, with contrast render to the base of the walls and window sills. Additional structures have been installed to the original structure, including aluminium security screens. A small brick and iron shed is located in the rear yard of the property, originally used as the external toilet. Two original brick chimneys are located towards the rear of the building. The dwelling originally consisted of three bedrooms, dining room, lounge, kitchen and bathroom, opening up onto a rear verandah. Two of the bedrooms had brick fire places, with additional fire places on the other side of the internal wall to the lounge and dining room. Since the site's discontinuance as a residence, modifications of the internal layout have been made, with several internal walls removed.

Several established gum trees are located on the site, to the rear of dwelling, adjacent to the Albany Highway road reserve. Additional fixtures to the building include two external air-conditioning condenser outlets (visible from the western elevation on the roof and wall of the northern elevation) and gas hot water system (visible from southern elevation). Openings on the southern elevation of the building (fronting Albany Highway) have been boarded up to prevent vandalism and illegal occupation of the site.

The site is currently enclosed by galvanised chain mesh fencing.

Condition	Fair
Integrity	Low
Authenticity	Medium
Date of Survey	12/12/2018



MRA
 Appendix 1

HISTORICAL INFORMATION

Following the granting of Responsible Government to Western Australia in 1890, greater levels of government borrowing were available to fund public works. The South Western Railway, which included a line from Perth to Armadale, was part of the funded works, with the line opening in 1893. The Kelmscott station was opened in 1893, originally consisting of a timber station and a goods yard to the rear. The need for a residence to house the station master was only determined in 1898, with a tender for the Kelmscott and Armadale (S.W.R) Station Master’s Houses Contract advertised by the State Railway Department on 24 February 1898, which included the Kelmscott Station. The Station Masters Residence was completed in 1905, with Mr T L Warner becoming the first Station Master to occupy the dwelling. The resident Kelmscott Station Master and Officer in Charge occupied the dwelling until 1970. Since the site ceased being a residence, the site has been used for a variety of commercial purposes, with former tenants including a real estate agency and an op-shop. In 2018 the building was unoccupied.

Historic Theme	Social & Civic Activities: Government & politics
Associations	Public Works Department
Sources	<ul style="list-style-type: none"> • Department of Planning Lands and Heritage Inherit Database • MRA Armadale Heritage Listing 2017 • City of Armadale (1995). Municipal Heritage Inventory 1995 Heritage and Conservation Professionals (2006) Kelmscott Town Centre – Cultural • Heritage Explanatory Report • “South Western Railway - Kelmscott and Armidale (sic) Station Masters’ Houses Contract”, The West Australian (Perth, WA -1879-1954), p.2, available from http://trove.nla.gov.au/newspaper/article/3196591 (accessed 25 February 2016)

STATEMENT OF SIGNIFICANCE

The Old Station Master’s Residence, a single storey Federation Bungalow, has cultural heritage significance for the following reasons:

- The place has aesthetic value as the last remaining dwelling in Western Australian that was built to house a station master and is one of the few remaining structures outside of Perth related to the Perth to Armadale railway line opened in 1893.
- The place has historic value as it was built by the Public Works Department under the State’s then Chief Architect, George Temple Poole.

Management Category	1
Level of Significance	Exceptional



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Appendix 1

ADDITIONAL PHOTOGRAPHS



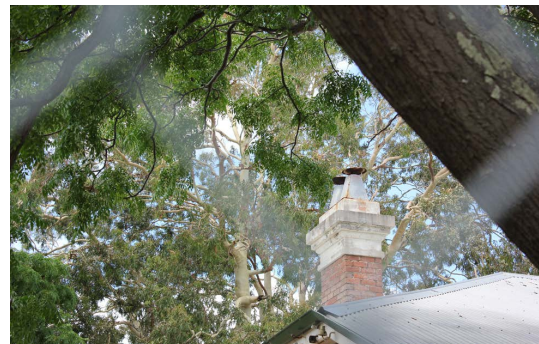
Symmetrical front verandah to the west elevation.
Source: SCA 12/12/2018



Boarded element to the east elevation.
Source: SCA 12/12/2018



Face brick to the north elevation.
Source: SCA 12/12/2018

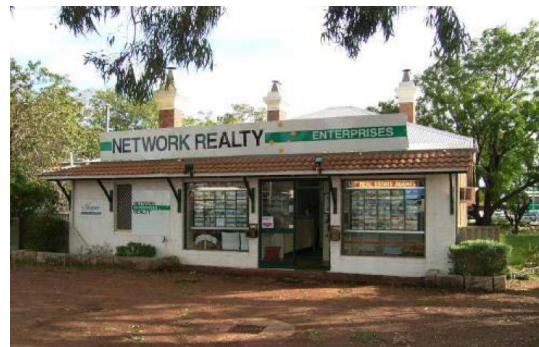


Masonry chimney with rendered capping.
Source: SCA 12/12/2018

HISTORICAL PHOTOGRAPHS



The Station Masters Residence, as seen from
Kelmscott Railway Station entry road (2016)
Source: 2017 MRA MHI



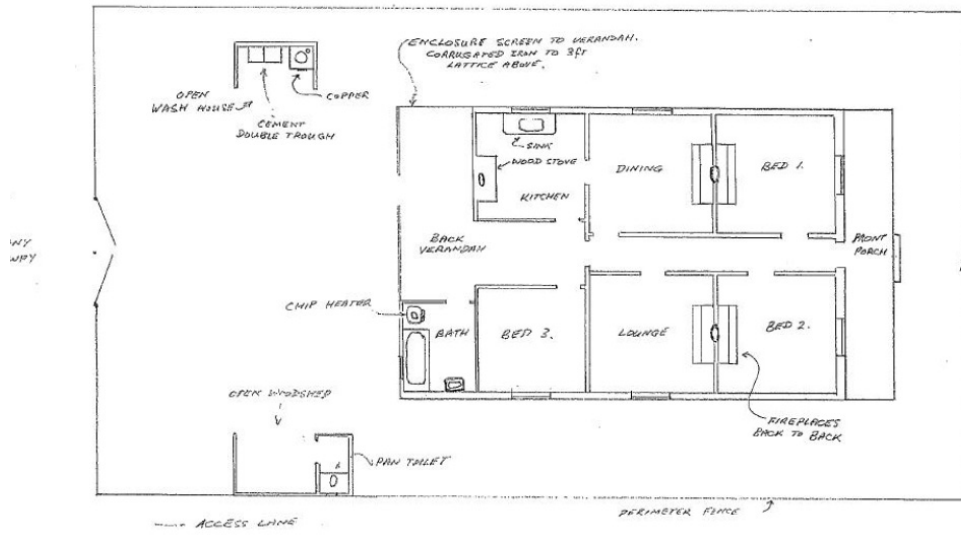
Old Station Masters Residence, as seen from
the rear elevation, 2005 (Source: Heritage and
Conservation Professionals)
Source: 2017 MRA MHI



MRA
Appendix 1



Building when formerly used as an Op Shop
Source: Elaine de Ruiter



Internal Plan Of The Kelmscott Station Masters Residence, 1950 (Source: Armadale District Heritage Group)
Source: MRA 2017



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The City of Armadale and its consultants have prepared the Local Heritage Survey (LHS) Review. Whilst the City takes care to ensure this information is accurate, the City cannot guarantee that the information is accurate or up to date. It has been compiled from a variety of different sources and may be subject to change from time to time without notice at the discretion of the Council or the City. Viewers must take account of this fact when assessing published information. If it is intended to rely on any of the information that the City provides in this document then the information should be verified independently.

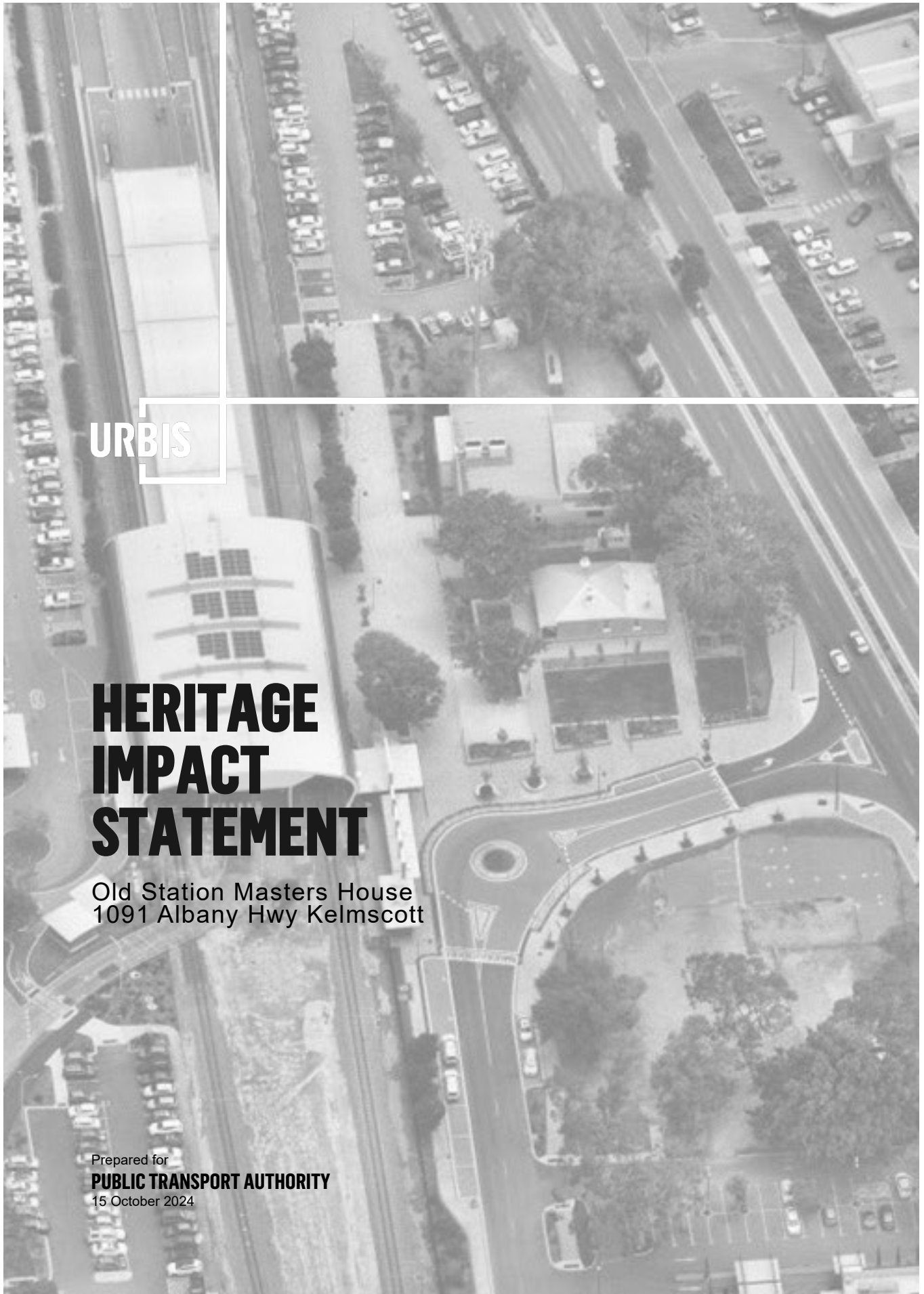
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URBIS

HERITAGE IMPACT STATEMENT

Old Station Masters House
1091 Albany Hwy Kelmscott

Prepared for
PUBLIC TRANSPORT AUTHORITY
15 October 2024

URBIS STAFF RESPONSIBLE FOR THIS REPORT WERE:

Director	Marc Beattie, MA (Hons), MSc IHBC, ICOMOS
Senior Consultant	Isabella Bellotti, M. Architecture, BA Design (Architecture)
Consultant	Vanessa Chapman
Project Code	P0054788
Report Number	01_FINAL 15/10/2024

Urbis acknowledges the important contribution that Aboriginal and Torres Strait Islander people make in creating a strong and vibrant Australian society.

We acknowledge, in each of our offices, the Traditional Owners on whose land we stand.

All information supplied to Urbis in order to conduct this research has been treated in the strictest confidence. It shall only be used in this context and shall not be made available to third parties without client authorisation. Confidential information has been stored securely and data provided by respondents, as well as their identity, has been treated in the strictest confidence and all assurance given to respondents have been and shall be fulfilled.

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1. EXECUTIVE SUMMARY

Urbis has been engaged by Plus Architecture on behalf of the Public Transport Authority (PTA) to prepare the following Heritage Impact Statement for the development of The Kelmscott Station Masters House (hereafter referred to as 'the site' or 'the subject site'). This Heritage Impact Statement will assess the potential impacts of the proposed works on the cultural heritage significance of the place.

The subject site is located on the Armadale Redevelopment Area Heritage Inventory as *Old Station Masters Residence* Place no.03479.

This Heritage Impact Statement will accompany the Development Application (DA) seeking consent for the proposed works.

A detailed impact assessment has been undertaken in Section 6. Overall, this report has found the following:

- The former Kelmscott Station Masters House has aesthetic and historical significance to the City of Armadale.
- The immediate context of the building has changed losing its private residential setting as it is now incorporated into the Kelmscott Station public open space.
- The proposed security upgrades and compliance works are required to provide universal access and toilets, prevent vandalism and encourage a future tenant and long-term use. Currently the buildings exposed location within the Kelmscott Train Station precinct, and vacancy has attracted antisocial behaviour and vandalism to the building's significant heritage fabric.
- The proposed UAT, security screens and ramping will have a minor visual impact on the aesthetic significance of the place but are modest and visually recessive to the finer architectural detailing of the former Station Masters House.
- The proposed works generally comply with heritage policy of the 2019 Conservation Management Plan, MRA Armadale Redevelopment Area Development Policy 2 Heritage Places. and ICOMOS Burra Charter Articles and can be readily removed in the future with no adverse physical impact to the places noted heritage fabric or values.

For the reasons stated above, the proposed works may be supported from a heritage perspective.

2. INTRODUCTION

2.1. BACKGROUND

Urbis has been engaged Plus Architecture on behalf of the Public Transport Authority to prepare the following Heritage Impact Statement for the development of the Kelmscott Station Masters House (hereafter referred to as 'the site' or 'the subject site'). This Heritage Impact Statement will assess the potential impacts of the proposed works on the cultural heritage significance of the place.

The subject site is located on the Armadale Redevelopment Area Heritage Inventory as *Old Station Masters Residence* Place no.03479.

The proposed development designed by Plus Architecture in consultation with the heritage team at Urbis, seeks adaptive reuse of the former Kelmscott Station Masters House including internal office fitout, verandah adaptations and UAT Toilet addition.

The Heritage Impact Statement will accompany the Development Application (DA) seeking consent for the proposed works.

Utilising the Heritage Council of Western Australia (HCWA) Guidelines, this heritage Impact Statement assessed the potential heritage impacts against the cultural heritage significance attributed to the site by the HCWA and the City of Armadale.

2.2. SITE LOCATION

The subject site is located at 2793 Albany Highway, Kelmscott within the Kelmscott Train Station precinct.



Figure 1: Aerial photograph of Kelmscott Station Masters House indicated in red.

Source: Nearthmaps 2024 with Urbis Overlay

2.3. METHODOLOGY & RELEVANT DOCUMENTS

This Heritage Impact Statement has been prepared in accordance with the HCWA Guidelines and utilises the statements of significance for the site by the City of Armadale. The philosophy and process adopted is guided by The Burra Charter: the Australia ICOMOS Charter for Places of Cultural Significance, 2013.

The proposed works have been assessed in accordance with the guiding document prepared by the Department of Planning, Lands and Heritage (DPLH) entitled Heritage Impact Statement – A Guide (HCWA, 2012) to address the following questions:

- How will the proposed works affect the heritage significance of the place?
- What measures (if any) are proposed to ameliorate any adverse impacts?
- Will the proposal result in any heritage conservation benefits that may offset any adverse impacts?

The following documents have informed the preparation of this Heritage Impact Statement:

- City of Armadale, Local Heritage Survey, Adopted 22 May 2023.
- DPLH, Kelmscott Station Masters House, Conservation Management Plan, June 2019
- Plus Architecture, Kelmscott Train Station Masters House Renovation, Development Application set 04/09/2024

2.4. AUTHOR IDENTIFICATION

This report has been prepared by Isabella Bellotti (Senior Heritage Consultant) and Vanessa Chapman (Consultant) Marc Beattie (Director, Heritage) has reviewed and endorsed the report.

Unless otherwise stated, all drawings, illustrations and photographs are the work of Urbis.

3. SITE CONTEXT

The former Station Masters House is bound by Albany Highway to the east, Streich Avenue to the south and the Kelmscott Train Station to the west. The Kelmscott exchange building is located north. The site is located within an industrial and commercial context with the Kelmscott Stargate Shopping centre across the road on the east side of Albany Highway.

The train station precinct around the building has been recently redeveloped and incorporating the building into the precinct with publicly accessible landscaping which includes paved pathways, accessible ramping, plantings and public seating.

A skillion roofed redbrick outbuilding is located on the north boundary.

The single storey redbrick building, constructed in 1905 is orientated with its principal façade facing the train station to the west. The building which has characteristics of the Victorian Georgian style is constructed in red face brick with concrete plinth and window sills. The dwelling has a rectangular plan with hipped roof form clad in corrugated metal sheeting which extends over the west verandah. The west verandah features a simple timber fascia to the front and infilled to the sides with timber batten panels. A Skillion verandah is located on the east. The roof features three tall brick and render chimneys. The verandahs have timber decking flooring and timber verandah posts and fretwork. Galvanised steel eaves gutters in a O-Gee profile line the roofs eaves and feature round galvanised steel downpipes. The downpipes are in varying condition. Some have been cut off at the base and are no longer attached to the adjacent drainage.

The west facades verandah has timber flooring. A central timber framed door with side and fan lights is located centrally with windows either side. Windows are timber framed two panel and double hung with soldier course lintels and concrete sills.

Internally the original plan consisted of three bedrooms, dining room, lounge kitchen and bathroom with a rear open verandah. Former bedrooms and dining/living feature fireplaces.

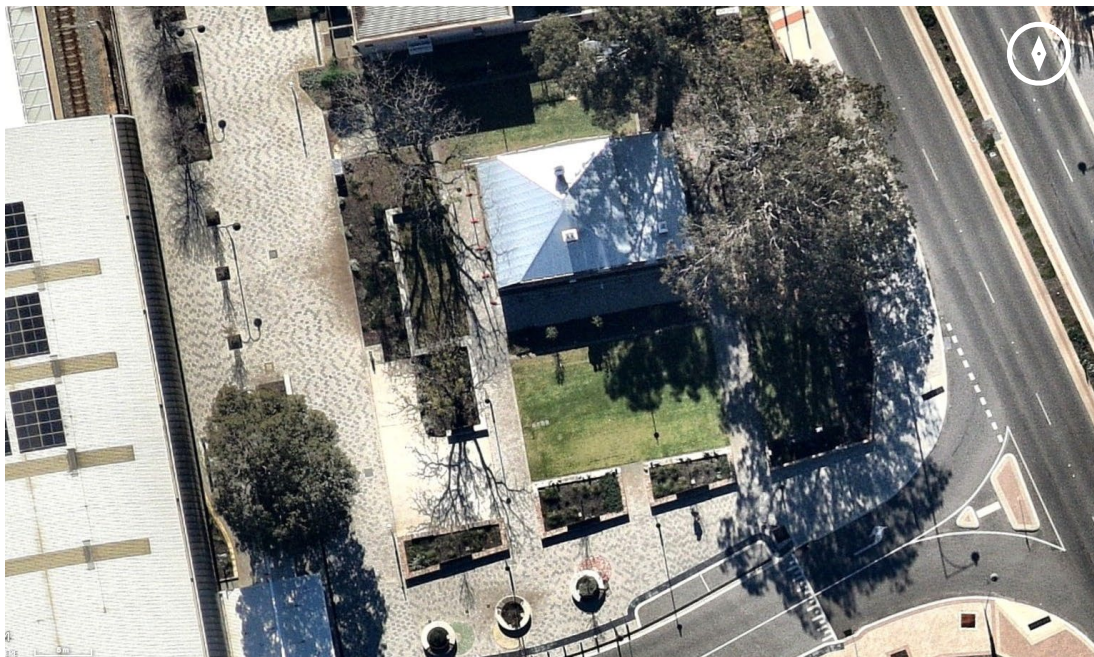


Figure 1: Aerial photograph of the existing condition of the site.
Source: NearMaps 2024



Figure 2 : Kelmscott Station Masters house north and west elevations looking south.

Source: Plus Architecture 2024



Figure 3 :Kelmscott Station Masters House west and south elevations looking north.

Source: Plus Architecture 2024



Figure 4 : Kelmscott Station Masters House west verandah.

Source: Plus Architecture 2024



Figure 5 :Kelmscott Station Masters House east verandah.

Source: Plus Architecture 2024



Figure 6 : Kelmscott Station Masters House east and south elevations looking north.

Source: Plus Architecture 2024



Figure 7 :Existing condition of west verandah rainwater goods. The foot of this DP is missing and disconnected from the drain.

Source: Plus Architecture 2024

4. HISTORICAL OVERVIEW

4.1. PRE-SETTLEMENT HISTORY

Kelmscott Station Masters House is located on Whadjuk Noongar Country. The nearest Aboriginal Cultural Heritage site to the Station Masters House is the Canning River, a creation/dreaming narrative site that was a source of water and ochre, located approximately 250 meters to the east. It is also located in close proximity to several recorded artefact scatters. This indicates that prior to settlement, the area was likely well-traversed by Noongar people as they seasonally travelled to and camped near the river.

4.2. KELMSCOTT STATION MASTERS HOUSE

Kelmscott was gazetted in 1830, and was one of the first townships to be established in the Swan River Colony.¹ In 1836, the road between Perth and Albany was established, running through Kelmscott.² The Albany road was important in the growth of Kelmscott for its role in the transportation of timber, mail, produce, and other goods as it represented a shift in the location of the Kelmscott settlement from the Swan River to being centred along the main road.³ The gold boom of the 1880s and 1890s further contributed to the growth of Kelmscott both in population and agricultural and farming production.⁴

The South Western Railway line from Perth to Armadale opened in 1893. The Kelmscott Station opened the same year, originally consisting of a timber station with a goods yard to the rear.⁵ It was determined in 1898 that a residence to house the station master of Kelmscott Station was needed, and the State Railway Department advertising a tender for the Kelmscott and Armadale (S.W.R) Station Master's Houses Contract on 24 February 1898.⁶ The station had actually been staffed since October 1887, however the officer in charge at that time had not been housed in a purpose built residence.⁷ The role of a station master included selling tickets, being on the platform constantly during the hours of business for the arrival and departure of trains, ensuring train stops were as short as possible, daily inspection of points and crossings, and maintain the appearance of the railway precinct itself.⁸

The tender for the Kelmscott Station Masters Residence was awarded to J.H. Brown for £589 2s. 2d. and it was completed in July of 1898.⁹ The first Station Master who may have occupied the residence was Sidney Glew, who worked the role from either 1897 or 98¹¹ to 1899¹², before the role was taken over by J Hayman until 1900.¹³ Mr Hicks was the Station Master from 1900 until 1904.¹⁴ After Mr Hicks, Lewis Warner was the Station Master for only a few years, from 1905 to 1908. In 1908 Stephen E Jones is listed as station

¹ Heritage and Conservation Professionals 2005, *Kelmscott Town Centre: Cultural Heritage Strategy Explanatory Report*, prepared for Armadale Redevelopment Authority, p. 7.

² Heritage and Conservation Professionals 2005, *Kelmscott Town Centre: Cultural Heritage Strategy Explanatory Report*, prepared for Armadale Redevelopment Authority, p. 8.

³ Midland Redevelopment Authority 2017, *Armadale Redevelopment Area, Heritage Inventory*, Perth, Western Australia, p. 13.

⁴ Heritage and Conservation Professionals 2005, *Kelmscott Town Centre: Cultural Heritage Strategy Explanatory Report*, prepared for Armadale Redevelopment Authority, pp. 9-10.

⁵ City of Armadale 2023, *Local Heritage Survey*, Armadale, Western Australia, p. 699.

⁶ City of Armadale 2023, *Local Heritage Survey*, Armadale, Western Australia, p. 699.

⁷ Austin, J 2011, *Station Masters of Western Australia*, Rail Heritage WA, Perth, Western Australia, p. 75.

⁸ Austin, J 2011, *Station Masters of Western Australia*, Rail Heritage WA, Perth, Western Australia, pp. 5-6.

⁹ *Government Gazette* 1898, 'W.R. - No. 2185/98 Western Australian Government Railways', 15 April, p. 945.

¹⁰ 'Western Australian Government Railways. Ways and Works Department. Minutes of Chief Engineer of Existing Lines Conference', 21 and 22 July 1989.

¹¹ *Wise's Post Office Directory* 1898, p. 174, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1898/0121.pdf>>.

¹² *Wise's Post Office Directory* 1899, p. 169, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1899/0121.pdf>>.

¹³ *Wise's Post Office Directory* 1900, p. 160, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1900/0111.pdf>>.

¹⁴ *Wise's Post Office Directory* 1904, p. 186, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1904/0123.pdf>>.

master, presumably for Kelmscott.¹⁵ The following year, 1909, Jos. Farnham is listed as station master.¹⁶ He appears to have fared much better than his predecessors in the role, remaining station master for Kelmscott until 1919.¹⁷ After Farnham, A E Whitney is listed as station master from 1919¹⁸ to 1922.¹⁹ Thos Olsen was station master from 1923²⁰ to 1925.²¹ J Arnott was station master from 1926²² until 1949.²³ When Mr Arnott finished his extensive tenure as Kelmscott Station Master, a farewell party was thrown for him and his wife as well respected members of the Kelmscott community before they moved to a different district.²⁴

The residence was occupied by the resident Kelmscott Station Master and Officer in Charge until 1970. The last station master for the Kelmscott Railway Station was Robert (Ted) Cox, who served as Station Master from circa 1950 until 1970, at which point the position was abolished by the Western Australian government.²⁵ As part of his position as Station Master, Ted Cox wore a uniform which included a cap²⁶ and a pith helmet (summer only)²⁷, which were donated to the Armadale Library. Nola Jones, Ted Cox's daughter, recalls in an oral history interview her father's time as the Kelmscott Station Master:

"Dad [Ted Cox] used to go [to work] early in the morning. He used to leave home about six, which was before I went to school, and he used to finish before I came home... Don Pike was on the afternoon shift. Dad was on the early morning to afternoon and then there was Don Pike."²⁸

It was at the time that the position of Station Master was abolished that the Western Australian Government Railways decided to cease providing housing for its employees and started disposing of its properties.²⁹ After this time the site ceased being a residence, and instead was used for a variety of commercial purposes. Former tenants during this time include a real estate agency and an op-shop.³⁰

¹⁵ *Wise's Post Office Directory* 1908, p. 209, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1908/0130.pdf>>.

¹⁶ *Wise's Post Office Directory* 1909, p. 199, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1909/0126.pdf>>.

¹⁷ *Wise's Post Office Directory* 1918, p. 139, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1918/0085.pdf>>.

¹⁸ *Wise's Post Office Directory* 1919, p. 137, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1919/0083.pdf>>.

¹⁹ *Wise's Post Office Directory* 1922, p. 135, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1922/0089.pdf>>.

²⁰ *Wise's Post Office Directory* 1923, p. 142, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1923/0092.pdf>>.

²¹ *Wise's Post Office Directory* 1925, p. 142, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1925/0095.pdf>>.

²² *Wise's Post Office Directory* 1926, p. 254, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1926/0149.pdf>>.

²³ *Wise's Post Office Directory* 1949, p. 389, State Library of Western Australia, Perth, Western Australia, viewed 23 August 2024, <<https://slwa.wa.gov.au/pdf/battye/pods/1949/0215.pdf>>.

²⁴ *South Western Advertiser* 1949, 'Farewell to Popular Kelmscott Couple', 22 September, Trove, viewed 23 August 2024, <<https://trove.nla.gov.au/newspaper/article/149760148?searchTerm=Kelmscott%20Station%20Master%20Arnott>>.

²⁵ Armadale Library 2024, *Cap – Western Australian Government Railway Station Master*, Perth, Western Australia, viewed 23 August 2024, <<https://armadale.spydus.com/cgi-bin/spydus.exe/FULL/OPAC/ALLWRKENQ/12295729/5297472,1>>.

²⁶ Armadale Library 2024, *Cap – Western Australian Government Railway Station Master*, Perth, Western Australia, viewed 23 August 2024, <<https://armadale.spydus.com/cgi-bin/spydus.exe/FULL/OPAC/ALLWRKENQ/12295729/5297472,1>>.

²⁷ Armadale Library 2024, *Pith Helmet – Western Australian Government Railways*, Perth, Western Australia, viewed 23 August 2024, <<https://armadale.spydus.com/cgi-bin/spydus.exe/FULL/OPAC/ALLWRKENQ/12295729/5299015,4>>.

²⁸ City of Armadale n.d., *Oral History Interview – Edited Transcript of an Interview with Nola Jones*, Birtwistle Local History Collection, interviewed by Peter Stocker, transcribed by Val Holmes, viewed 23 August 2024, <<https://armadale.spydus.com/cgi-bin/spydus.exe/FULL/OPAC/BIBENQ/12296021/7775070,1>>.

²⁹ Menck, C 2014, *A Thematic History of Government Housing in Western Australia*, Department of Housing, Perth, Western Australia, p. 328.

³⁰ City of Armadale 2023, *Local Heritage Survey*, Armadale, Western Australia, p. 699.

In the 1970s, the original Kelmscott railway station buildings were demolished to make way for a new bus and rail transfer station,³¹ and further road widening and redevelopment of the Albany Highway at this time also resulted in the demolition of several heritage buildings in the vicinity.³²

In 2018, the Western Australian Government announced plans to redevelop the area, including the refurbishment of the Station Masters House and its re-purpose for a new use.³³

The building has been vacant since 2014.³⁴

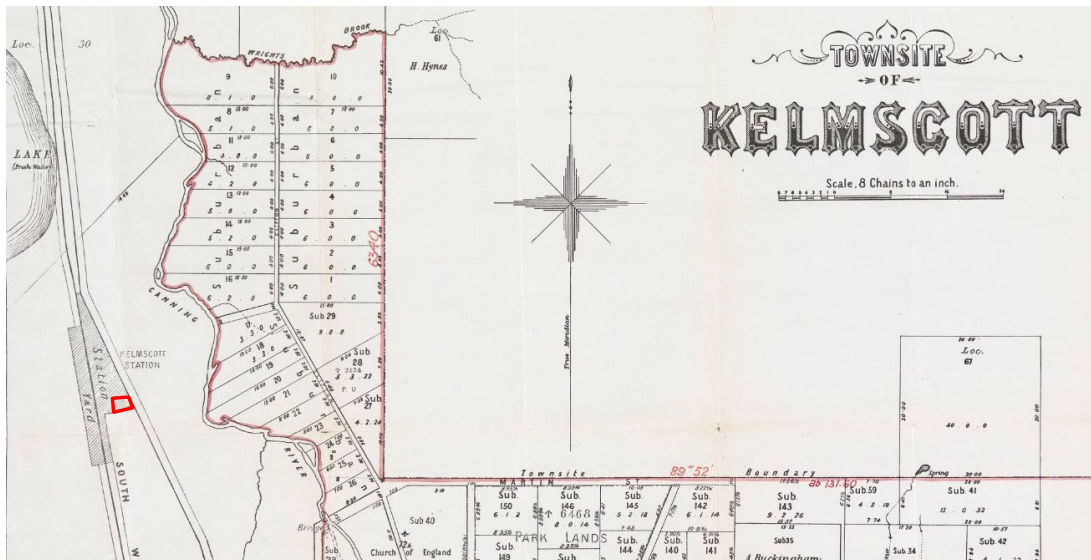


Figure 8 Map of Kelmscott 1899. With approximate location of subject site highlighted in red.

Source: Adams, J 1899, *Townsite of Kelmscott*, 8 chains to an inch, Location 31, State Library of Western Australia, Perth, Western Australia, viewed 23 August 20204, <https://purl.slwa.wa.gov.au/slwa_b1902332_1>.

³¹ Heritage and Conservation Professionals 2005, *Kelmscott Town Centre: Cultural Heritage Strategy Explanatory Report*, prepared for Armadale Redevelopment Authority, p. 41.

³² Heritage and Conservation Professionals 2005, *Kelmscott Town Centre: Cultural Heritage Strategy Explanatory Report*, prepared for Armadale Redevelopment Authority, p. 16.

³³ Heritage Works 2019, *Kelmscott Station Master's House Conservation Management Plan*, Department of Planning, Lands and Heritage, Perth, Western Australia, p. 13.

³⁴ Heritage Works 2019, *Kelmscott Station Master's House Conservation Management Plan*, Department of Planning, Lands and Heritage, Perth, Western Australia, p. 13.



Figure 9 Kelmscott Station 1921.

Source: Amateur photographer 1921, *Kelmscott railway station*, Armadale Library, Perth, Western Australia, viewed 23 August 2024, <<https://armadale.spydus.com/cgi-bin/spydus.exe/ENQ/OPAC/BIBENQ/12303206?SETLVL=&IRN=333485&NAVLVL=SET>>.



Figure 10 Kelmscott Station 1920.

Source: Amateur photographer 1920, *Kelmscott railway station*, Armadale Library, Perth, Western Australia, viewed 23 August 2024, <<https://armadale.spydus.com/cgi-bin/spydus.exe/ENQ/OPAC/BIBENQ/12303206?SETLVL=&IRN=342602>>.



Figure 11 Aerial photograph 1965.

Source: *Landgate Map Viewer Plus*.



Figure 12 Aerial photograph 1970.

Source: *Landgate Map Viewer Plus*.



Figure 13 Aerial photograph 1981.

Source: *Landgate Map Viewer Plus*.



Figure 14 Aerial photograph 1989.

Source: *Landgate Map Viewer Plus*.



Figure 15 Aerial photograph 2000.

Source: Landgate Map Viewer Plus.



Figure 16 Aerial photograph 2005.

Source: Landgate Map Viewer Plus.



Figure 17 Aerial photograph 2013.

Source: Landgate Map Viewer Plus.



Figure 18 Aerial photograph 2020.

Source: Landgate Map Viewer Plus.



Figure 19 Aerial photograph 2021.

Source: Landgate Map Viewer Plus.



Figure 20 Aerial photograph 2024.

Source: Landgate Map Viewer Plus.

5. HERITAGE SIGNIFICANCE

5.1. HERITAGE LISTINGS

The site has been recognised as a place of cultural heritage value through inclusion on the following statutory heritage lists.

Table 1: Kelmscott Station Masters House heritage listings

No.	Place or item	Description	Notes
City of Armadale Heritage Survey (As part of the Armadale Redevelopment Authority/ DevelopmentWA)			
03479	<i>Kelmscott Station Masters House</i>		Management Category 1 (Exceptional level of significance)

Source: HCWA

5.2. STATEMENT OF SIGNIFICANCE

The Old Station Masters House has been assigned the following statement of significance by DevelopmentWA:

The place has aesthetic value as the last remaining dwelling in Western Australian that was built to house a station master and is one of the few remaining structures outside of Perth related to the Perth to Armadale railway line opened in 1893. The place has historic value as it was built by the Public Works Department under the State's then Chief Architect, George Temple Poole.³⁵

5.3. CONSERVATION MANAGEMENT PLAN & GRADING OF SIGNIFICANCE

A Conservation Management Plan (CMP) was written in 2019 for the site by DPLH to guide future use, care management and redevelopment of the place. This included an assessment of significance and a grading of individual zones of significance.

Note this document was written prior to the Denny Avenue Level Crossing works in 2020/2021 which has changed the immediate context of the house from a fenced residential block to an open planned landscaping with public views from Albany Hwy and Steich Avenue. The building has also since been restored with some elements listed as intrusive already removed.

The CMP has designated the following zones of significance which have been prepared on the basis of state significance. Overall the place has been identified as having an exceptional local significance.

Table 2: Relative grading of significance

Heritage Significance	Definition	Element or architectural component
Exceptional Significance	<i>Items that would warrant inclusion on any register of heritage places when assessed at a national level. Conservation of items of exception significance is essential.</i>	Nil
Considerable Significance	<i>Items that when assessed in a state context would warrant inclusion on any register of heritage places. Conservation</i>	Nil

³⁵ City of Armadale, Local Heritage Survey, 2023

Heritage Significance	Definition	Element or architectural component
	<i>of items of considerable significance is highly recommended.</i>	
Some Significance	<i>Items which are at the threshold for entry onto registers of significance, at a State or local level. Conservation of items of some significance is recommended.</i>	Original form and planning, including roof form. Original fabric
Little Significance	<i>Items which neither contribute to nor detract from the cultural significance of the place, and are often later interventions. Items of little significance may be retained or removed as required.</i>	Floor coverings Window treatments
Intrusive Elements	<i>Elements which intrude visually or negatively impact on the cultural significance of the place. Intrusive elements may be removed as the opportunity arises.</i>	Infill to fireplace, G.07 Window screens Later internal modifications and fitout Modified openings to west elevation (removed) Tiled portion of roof over west elevation (removed) Awning over opening on south elevation Fencing (removed) Air conditioning units Light fittings

6. THE PROPOSAL

6.1. PROPOSED WORKS

The proposed works to the subject site as part of this DA are summarised in the table below.

The proposed works have been designed to respond and address the antisocial behaviour which the currently vacant property has attracted given its exposed location with the Kelmscott train station precinct. Proposed security upgrades and the installation of complaint universal access and toilets will encourage the long-term tenancy of the property supporting the properties enduring conservation.

Table 3 Proposed Works

Proposed works	Comments
Conservation works to downpipes.	<p>Spot repairs to damaged stormwater drainage such as downpipes, some of which are missing parts and disconnected from the drains. Items will be repaired and replaced on a like for like basis.</p> <p>If original, downpipes are graded as some significance.</p>
Construction of a DA complaint ramp and walkway to west verandah.	<p>An access ramp and stairs are proposed to access the front door (west elevation). It will be constructed with reclaimed red brick retaining walls and pavers to match the existing surrounding landscaping (completed in 2023) and metal hand rail to the stairs.</p> <p>The original form of the building which includes the west facade is graded as some significance.</p>
Construction of a pre-fabricated UAT with lightweight screening	<p>A prefabricated universally accessible toilet (UAT) is proposed for the north facade connected to the west verandah. It will be concealed behind a light weight timber batten screen.</p> <p>The original form of the building which includes the west facade is graded as some significance.</p>
Installation of continuous security screening to windows and both verandahs	<p>Security screening is proposed for both east and west verandahs and north and south windows. Window screens will be of a transparent wire mesh.</p> <p>Metal mesh panels will be fixed to a new timber batten verandah balustrade and will match the security screens.</p> <p>The original form of the building is graded as some significance while window treatments are of little significance.</p>

6.2. RELEVANT PLANS

This Heritage Impact Statement is intended to be read in conjunction with the architectural documents by Plus Architecture (dated 04/09/2024) and other documents submitted as part of this permit application.

Key extracts from the plans submitted to Urbis by Plus Architecture are included below. Please refer to the full DA documentation for detailed full-sized plans.

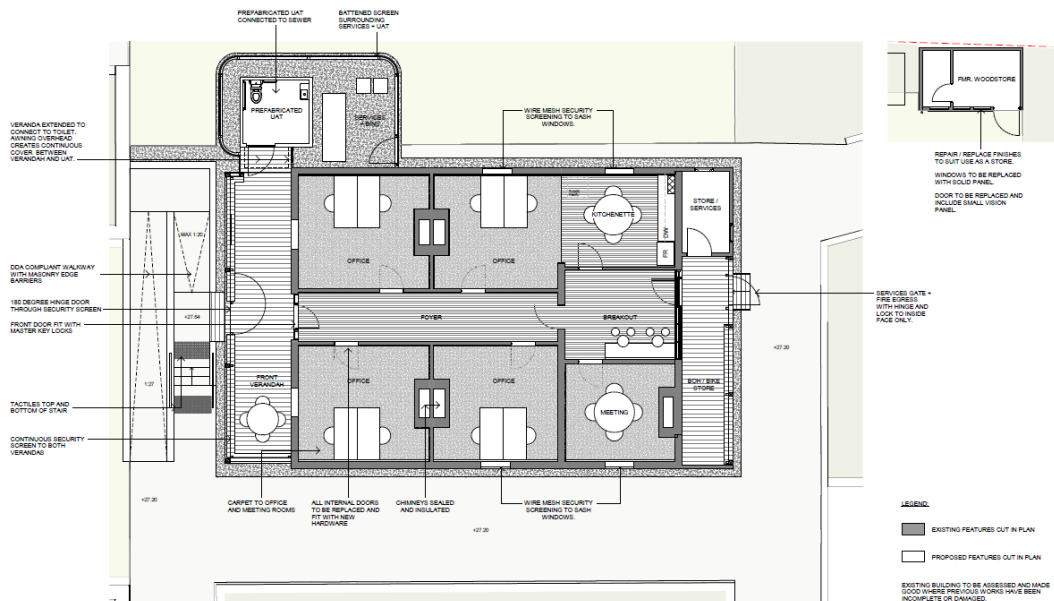


Figure 21 Proposed Floor Plan
 Source: Plus Architecture 04/09/2024

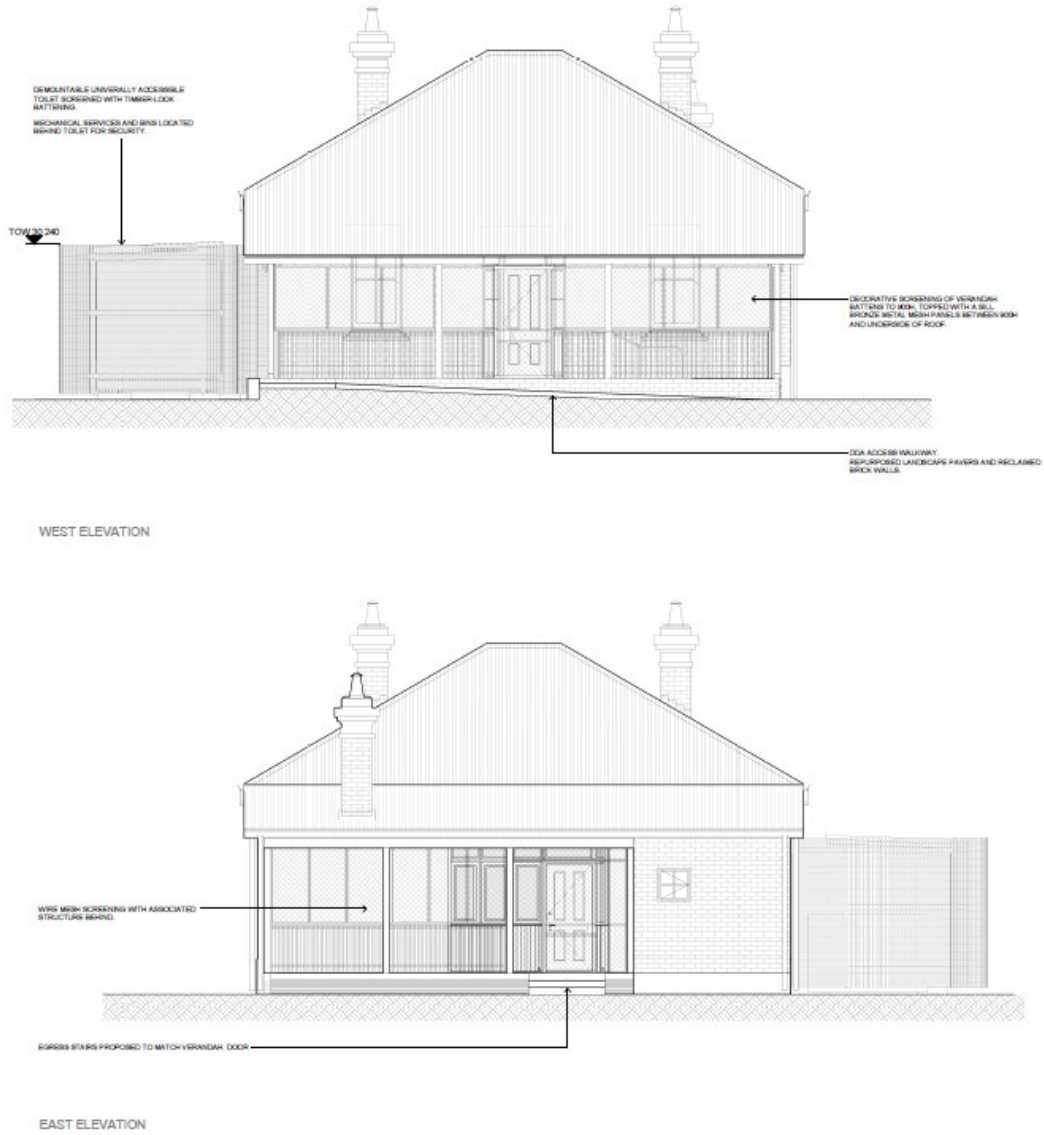


Figure 22 West and East elevations
Source: Plus Architecture 04/09/2024

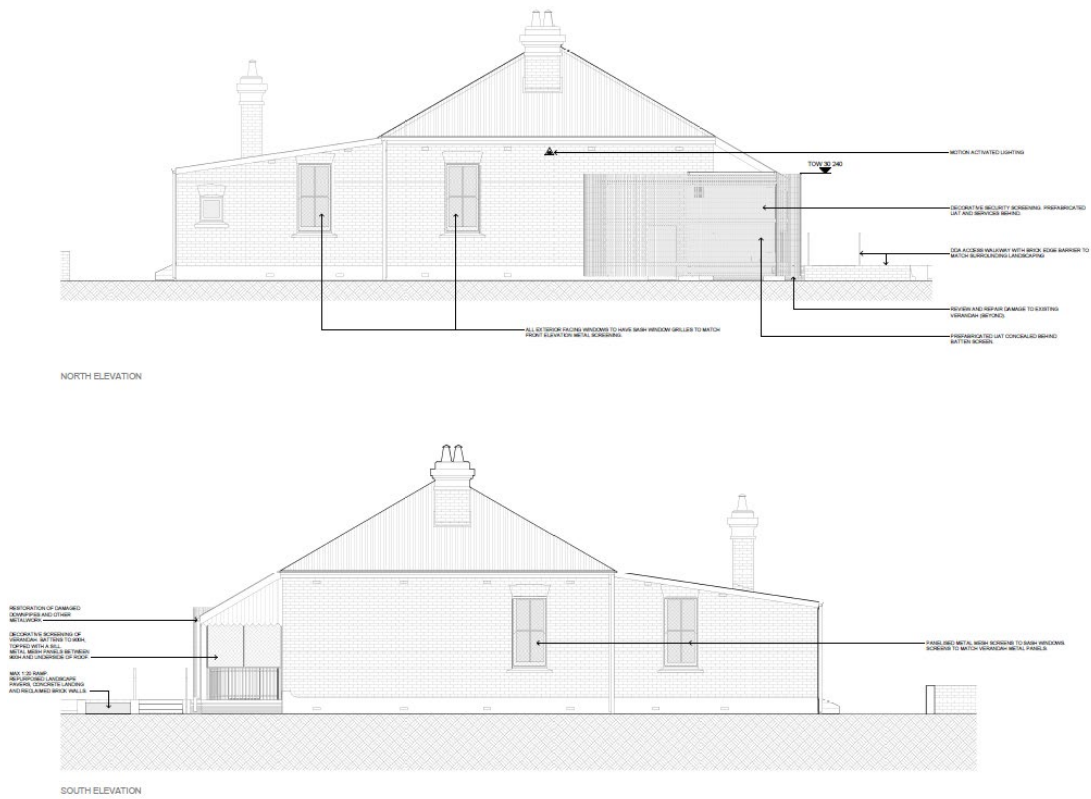


Figure 23 South and North elevations
Source: Plus Architecture 04/09/2024



Figure 24 3D view west elevation
Source: Plus Architecture 04/09/2024



Figure 25 3D view East elevation
Source: Plus Architecture 04/09/2024

7. ASSESSMENT OF HERITAGE IMPACT

The following impact assessment has assessed the potential heritage impacts of the proposed works to the former Kelmscott Station Masters House utilising the HCWA’s Statement of Heritage Impact” assessment guidelines.

Table 4: Assessment of Heritage Impact against the places Statement of significance

Significance	Potential Impact	Mitigation
<p><i>The place has aesthetic value as the last remaining dwelling in Western Australian that was built to house a station master</i></p>	<p>Minor Impact</p> <p>The proposed works will have a minor alteration to the buildings current aesthetic presentation by installing security screening to the verandah and windows, the construction of a UAT toilet and compliant universal accessible ramping to the principal west entrance.</p>	<p>The proposed alterations, while to the exterior of the building, will not detract from the building’s significant aesthetic value.</p> <p>The proposed security screens to the verandah and windows are required to address current vandalism and antisocial behaviour that impacts the building’s ability to be rented out to a tenant. The ongoing use of the building will positively benefit the conservation of the place by providing ongoing monitoring and maintenance works to conserve the heritage fabric.</p> <p>The screens and proposed timber balustrading to the front (west) verandah while a diversion from the original appearance of the place, are lightweight and maintain a level of transparency so that the buildings windows and verandah can still be readily interpreted from the public realm.</p> <p>The infill verandah balustrading will be timber with a modest contemporary profile which references a contextual typology of verandah balustrading while being clearly contemporary and legible as new in accordance with ICOMOS Burra Charter Article 15. Change.</p> <p>If the security requirements of the building change in the future following continuous occupation the screens can be readily removed due to lightweight self-supporting framing with little physical impact to the place’s heritage fabric.</p> <p>The proposed new accessible ramp and walkway provide compliant universal access into the building and the proposed new UAT and will be constructed in red brick, paving to the match the surrounding landscaping and a modest metal handrail. Materials complement the existing building, and the works will not physically impact the building heritage fabric. Allowing the original internal plan to be fully appreciated.</p> <p>The proposed UAT will be prefabricated, self-supporting and does not require the demolition of any</p>

Significance	Potential Impact	Mitigation
		<p>of the building's significant heritage fabric. It will be concealed behind a timber batten screen connected to one of the verandah posts. Following the surrounding train station landscaping works in 2020/2021 the immediate context of the building has changed with the south east and west aspect is now open to the Kelmscott Station landscaping. The north is the most concealed elevation of the building and is a suitable location for the proposed addition.</p> <p>While the UAT is a minor alteration to the principal west facade of the building, the modular architecture is modest and clearly gives hierarchy to the finer architectural detailing of the former station masters house. The structure is readily identifiable as a new addition and the buildings original form, scale and detailing still clearly legible.</p> <p>The building has already undergone significant alterations and subsequent restorations and is not entirely intact. The proposed works will maintain the overall form and appearance of the building conserving its significant representation as a former Station Masters House.</p> <p>All the works are fully reversible and there is sufficient documentary evidence of the buildings original condition and plan to inform an accurate restoration of the building in the future if required. The works generally comply with the MRA Armadale Redevelopment Area <i>Development Policy 2 Heritage Places</i>.</p>
<p><i>and is one of the few remaining structures outside of Perth related to the Perth to Armadale railway line opened in 1893.</i></p>	<p>No discernible impact</p> <p>The building will be retained in its full current form maintaining the structure as a representation of a building related to the Perth to Armadale railway line.</p>	
<p><i>The place has historic value as it was built by the Public Works Department under the State's then</i></p>	<p>Positive Impact</p> <p>The building will be retained in full</p>	

Significance	Potential Impact	Mitigation
Chief Architect, George Temple Poole.	maintaining its noted historical value. The proposed works will allow the building to be reused and the original plan of the place to be appreciated into the future.	

Table 5: Assessment of compliance with relevant CMP policy

Policy	Compliance	Comment
<i>Policy 3 : The policies, principles and processes of the Burra Charter should be used to guide conservation of the place, in addition to the policy outlined in this document.</i>	Complies	The proposed works are in accordance with relevant Burra Charter Articles 2,3,5,7,8 and 15.
<i>Policy 11: Fabric and elements identified as having some significance should be retained and conserved, where possible. If modified this should be done in keeping with the overall aims of the conservation policy. If removed, their significance should be recognised and recorded through an archival record</i>	Complies	The exterior form of the building is graded as some significance and will be retained and conserved in accordance with this policy. No demolition of heritage fabric is proposed.
<i>Policy 12: Fabric and elements identified as having little significance may be retained, removed or modified. If modified this should be done in keeping with the overall aims of the conservation policy</i>	complies	The buildings window treatments are graded as little significance. Existing windows will be retained and only modified externally with additive security screening.
<i>Policy 14 : Views of the Kelmscott Station Master's House should be considered during redevelopment of the Station generally and the Station Master's House generally. Views should not be obstructed by future development or landscaping. Principal views are of the front of the building, from the railway side, but</i>	Complies	The proposed UAT addition is located adjacent to the principal facade on the north elevation and does not obscure any existing views of the dwelling from Kelmscott Station. Views are also maintained to the secondary frontage to Albany Highway.

Policy	Compliance	Comment
<i>the views of the rear should also be considered given the volume of traffic on the highway</i>		
<i>Policy 21: The original form of Kelmscott Station Master's House should be retained and conserved</i>	Complies	No demolition is proposed, and the original form of the dwelling will be retained and conserved as part of the proposed works.
<i>Policy 26: Where required rainwater goods should be repaired/replaced to match existing materials, profiles and finishes</i>	Complies	Downpipes will be restored as required on a like for like basis to ensure functionality.
<i>Policy 29: Damage to verandah and steps to be made good to match existing. Compliance requirements for the steps should be investigated and implemented, with all work in a style to complement the dwelling</i>	Complies	The proposed new ramping and stairs to the front verandah provide compliant universal access to the public building. The works are low level and do not obscure any views of the dwelling and chosen materials such as recycled brick complimenting the existing materiality of the place.
<i>Policy 34: Original openings are of some significance and should be retained and conserved. Modifications should only be made where they are critical to the ongoing use of the place</i>	Complies	The existing openings will be retained and conserved. Mesh security screens are proposed to prevent vandalism and encourage a tenant, critical to the ongoing use of the place.
<i>Policy 37: Consideration should be given to removing the screens and installing less intrusive security measures, such as glazing films or stainless-steel mesh.</i>	Complies	The screens mentioned in the 2019 CMP have since been removed. New screens will be of a lightweight mesh and provide adequate visibility of the timber window frame behind.
<i>Policy 62: The place should be used to aid in its ongoing conservation.</i>	Complies	The proposed works will encourage a new tenant for the building.
<i>Policy 64: Changes required for future use should be minimised and easily reversible, and the original intent of the place should not be obscured. Values should not be obscured by change.</i>	Complies	All proposed works are readily reversible with no adverse physical impact to the dwelling's heritage fabric.
<i>Policy 66: Kelmscott Station Master's House must be retained and conserved. The area where development may occur in the immediate vicinity of the building is illustrated in Fig. 32 –(Future Development diagram (page 43)</i>	Not compliant	The CMP was written prior to the landscaping works which have resulted in the west, south and east elevations being open to the public as part of the Kelmscott Stations landscaping. The south and east are now the most visible elevations and are not a suitable location for additions. The north as proposed is more suitable given it is adjacent to an telecommunications

Policy	Compliance	Comment
		exchange. The UAT is located accordingly to the accessible ramp which services the principal entrance providing equitable access for the people of abilities and prevents any physical impacts to heritage fabric to accommodate the UAT internally or on the east facade.
<i>Policy 67: Any new development should be lightweight, clearly contemporary in design, and easily reversible. New structures should be located so as not to impinge on significant views of the place, or to require demolition of significant fabric.</i>	Complies	The proposed UAT addition is lightweight and self-supporting and of a sympathetic contemporary design as to not draw undue attention away from the finer architectural detailing of the former Station Masters House.
<i>Policy 68: New structures should be freestanding and reversible, with any connections to the existing buildings minimised</i>	Complies	All proposed new structures are self-supporting and can be readily removed with no adverse impact to heritage fabric.

8. CONCLUSION

This Heritage Impact Statement has assessed the potential impacts of the proposed development on the heritage significance of the former Kelmscott Station Masters House. The proposed works have been consciously designed to respond and address the antisocial behaviour which the currently vacant property has attracted given its exposed location with the Kelmscott train station precinct. Proposed security upgrades and the installation of complaint universal access and toilets will encourage the long-term tenancy of the property supporting the properties enduring conservation.

The report has found the following:

- The former Kelmscott Station Masters House has aesthetic and historical significance to the City of Armadale.
- The immediate context of the building has changed losing its private residential setting as it is now incorporated into the Kelmscott Station public open space.
- The proposed security upgrades and compliance works are required to provide universal access and toilets, prevent vandalism and encourage a future tenant and long-term use. Currently the buildings exposed location within the Kelmscott Train Station precinct, and vacancy has attracted antisocial behaviour and vandalism to the building's significant heritage fabric.
- The proposed UAT, security screens and ramping will have a minor visual impact on the aesthetic significance of the place but are modest and visually recessive to the finer architectural detailing of the former Station Masters House.
- The proposed works generally comply with heritage policy of the 2019 Conservation Management Plan, MRA Armadale Redevelopment Area Development Policy 2 Heritage Places. and ICOMOS Burra Charter Articles and can be readily removed in the future with no adverse physical impact to the places noted heritage fabric or values.

For the reasons statement above, the proposed works are recommended for approval from a heritage perspective.

9. REFERENCES

City of Armadale 2023, *Local Heritage Survey*, Armadale, Western Australia.

Heritage and Conservation Professionals 2005, *Kelmscott Town Centre: Cultural Heritage Strategy Explanatory Report*, prepared for Armadale Redevelopment Authority.

Heritage Works 2019, *Kelmscott Station Master's House Conservation Management Plan*, Department of Planning, Lands and Heritage, Perth, Western Australia.

Midland Redevelopment Authority 2017, *Armadale Redevelopment Area, Heritage Inventory*, Perth, Western Australia.

Wise's Post Office Directory 1898-1949.

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